

CHRISTLETON AND LITTLETON NEIGHBOURHOOD PLAN (NP) v31

Regulation 16 consultation by
Cheshire West and Chester Borough Council



This NP will take full effect from 2026 to 2042



this document is better viewed at:
<https://np.christleton-pc.gov.uk/documents/>
so that you can view the linked documents



CHRISTLETON AND LITTLETON NEIGHBOURHOOD PLAN (NP)

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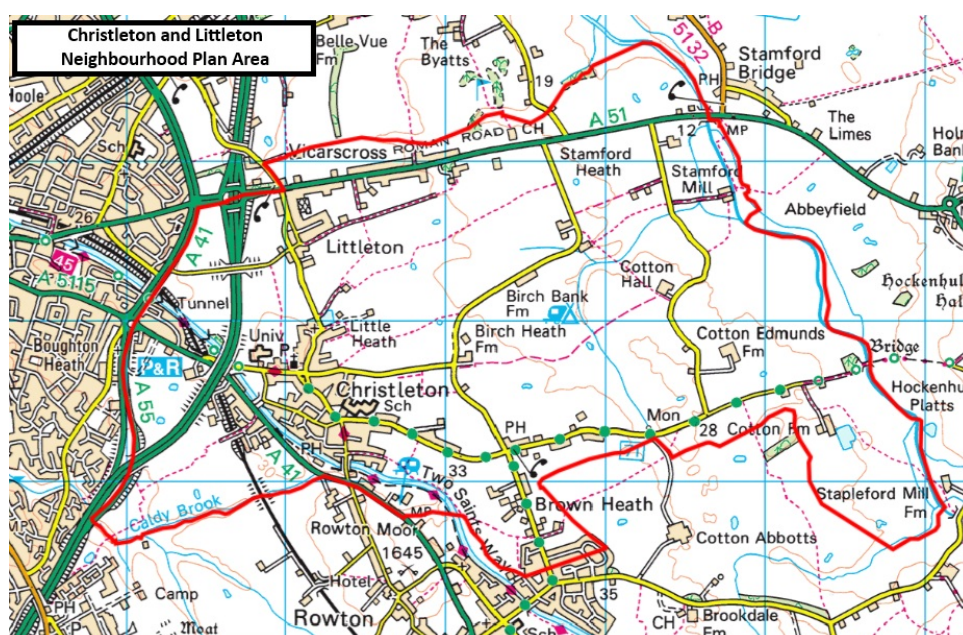
1.INTRODUCTION

The traditional villages of Christleton and Littleton are surrounded by working farmland and washed over by statutory green belt with supporting restrictive policies set out in CWaC adopted Local Plan part 1 and part 2. However, over several years an unchecked series of brownfield and infill developments have threatened the rural character of the villages by being too high, too large, too densely packed, eliminated open views, invaded privacy of adjacent housing, of a low quality and unsympathetic to the character and heritage of the villages.

Christleton and Littleton Parish Councils concluded in 2023 that they must influence development through a Neighbourhood Plan (NP) so as to avoid the persistently adverse impact of development on the villages. Cheshire West and Chester requested the Parish Council submit a Parish Priority Statement (PPS), a short form version of the NP, in advance of the NP to assist with preparation of the Local Plan. The PPS was provided in July 2025. Neighbourhood Planning came about as a result of the Localism Act 2011, with the aim of giving more power and a bigger voice in planning matters to local communities. The Neighbourhood Plan for Christleton and Littleton is a community project and resident involvement is vital to the process.

The Christleton and Littleton Neighbourhood Plan area was designated by Cheshire West and Chester Council following a consultation period that ran from the 14th of December 2023 to the 1st of February 2024. The application, submitted under Regulation 5 of Neighbourhood Plan regulations 2012, encompassed the areas covered by Christleton and Littleton Parish Councils, aligning exactly with their respective parish boundaries.

Once a Neighbourhood Plan is completed, it is subject to an independent examination process to assess whether it meets certain 'basic conditions' in accordance with the requirements of the regulations. These include the need to have regard to national policies and advice, to be in general conformity with the strategic policies in the Cheshire West and Chester Local Plan and to contribute to the achievement of sustainable development. If the basic conditions are met, a public referendum can then be held. If it is supported by residents, the plan is then 'made'. It becomes a legal policy document that must be taken into consideration by the planning authority for all planning applications in the area with the exception of mineral extraction applications. This NP will take full effect from the date on which it is made until the end of the next local plan period indicatively 2026-2042.



Christleton and Littleton Parish Councils invited and assembled a group of 20 volunteers. A series of briefing sessions were arranged and a final meeting organised at which it was decided to prioritise:

- appropriate housing needs for local people
- wildlife, ecology, biodiversity, trees, hedgerows
- green gaps, green spaces, incidental open spaces and conservation areas
- building design, local character and energy efficiency
- infrastructure including air quality, traffic, cycle paths, public rights of way, flood management and car parking
- heritage
- community facilities

The volunteers acquired funding from the Parish Councils and Localities and carried out a [VILLAGE CONSULTATION](#) which informed and heavily influenced the selected priorities.

Work started on assembling the components of the Christleton and Littleton Neighbourhood Plan and a Steering Group (NPSG) was formed from the volunteers assembling the Plan.

The NPSG completed the Christleton and Littleton Neighbourhood Plan v26 and carried out the required regulation 14 consultation from November 10 2025 to December 22 2025, which involved posting the plan to all residents, emailing all statutory consultees and posting notices on noticeboards, on the PC website and facebook so as to access all residents, businesses, landowners and developers. The comments received were evaluated by the NPSG, responded to, and incorporated into this v31 and is submitted here for a regulation 16 consultation by the Borough Council.

This Christleton and Littleton Neighbourhood Plan sets out a series of policies which will be used to guide the preparation and submission of planning applications across the plan area by:

- taking account of policies set out in [LOCAL PLAN part 1](#) and [LOCAL PLAN part 2](#) together with the recent changes proposed by Government in the National Planning Policy framework which recognise the importance of climate change and sustainability
- delivering a Vision for Christleton and Littleton up to 2042, to reflect the timescale of the Cheshire West and Chester Local Plan
- producing a Design Code for the plan area to supplement the emerging Cheshire West and Chester Design Guide for the wider area
- containing a number of Parish Council Actions which seek to address important issues across the plan area that cannot be included as policies within the plan but that are critically important to the local community

HYPERLINKS

Underlined hyperlinked documents can be found at the following NP website address
<https://np.christleton-pc.gov.uk/documents/> for reference and reviewing the paper plan:

[VILLAGE CONSULTATION](#)

[AIR QUALITY REPORT](#)

[LOCAL PLAN part 1](#)

[LOCAL PLAN part 2](#)

[HOUSING NEEDS SURVEY](#)

[HOUSING DENSITY](#)

[CHRISTLETON HERITAGE ASSESSMENT](#)

[CHRISTLETON HERITAGE TRAIL](#)

[BATTLE OF ROWTON HEATH 1645](#)

[SHROPSHIRE UNION CANAL POLICIES](#)

[CAR PARKING DESIGN](#)

[CHRISTLETON CYCLING WALKING AND TRAFFIC PLAN](#)

[HIGH SCHOOL TRAVEL SURVEY](#)

[SE CHESTER CYCLING PLANS](#)

[SE CHESTER CYCLING PLAN PRIORITIES](#)

[CHRISTLETON AND LITTLETON VIEWS](#)

[CHESHIRE WILDLIFE TRUST REPORT](#)

[WILDLIFE BACKGROUND DOCUMENT](#)

[WILDLIFE SURVEY](#)

2.HISTORY OF THE VILLAGES

Charles Smith. Smith's New English Atlas, 1804.

It is certain that buildings existed in what is now Christleton since Roman times, probably because the village is positioned on the route between the salt mining areas surrounding Middlewich and the port of Deva (Chester), home of the garrison of the twentieth legion. Salt was a vital commodity for the Romans and could be shipped elsewhere conveniently from Chester.



The first documented evidence comes from the Domesday Book (1086), when “Cristentone” is part of the Norman Barony of Malpas, and the property of Robert, the Norman earl. The unique name of Christleton is said to mean “The place of the Christians” or “a farmstead with a cross”. It is likely that a wooden church stood near the current church site during the late Saxon period, as the village is already well established by the Norman period, and one of the larger villages of Cheshire.

The term “the place of the Christians” may have been reinforced by the fact that Chester Cathedral was founded in the 10th century as a Benedictine abbey dedicated to St Werburgh and the areas around Christleton were the market gardens and farmsteads serving the monastery. The Abbot’s Well, at the site of today’s Mercure hotel on the A41 trunk road, was the source of a piped water supply to the abbey from the 13th century.

With the dissolution of the monasteries in the 16th century, the Ecclesiastical Parish of Christleton was formed with five townships: Christleton, Rowton (rough Christleton), Littleton (little Christleton), Cotton Abbots and Cotton Edmunds and remains this way till today.



The next century brought the Civil War and the Old Hall in the centre of Christleton village housed the Parliamentary headquarters during both the siege of Chester, which was a Royalist stronghold, the [BATTLE OF ROWTON HEATH 1645](#)ath was less than a mile from today’s village centre and is in the battlefields register under Rowton Heath which provides statutory protection. There are no complete buildings predating 1645 left standing as a result of this conflict, though the Old Hall itself dates from 1605 and is a timber framed encased in brick. The wider area of the of the Battle is bounded by Hatton Heath to the south, the

Shropshire Union Canal to the east and Manor Farm to the west. The centre of the Battle was bounded by the Shropshire Union Canal and Rowton Lane and is in Christleton as well as Rowton.

Christleton 1960s

The 18th century brought the canals to Christleton when the Chester Canal Company was established to bring goods for export from the industrial Midlands to the port on the River Dee. Running from Nantwich to Chester, the Chester Canal was never a commercial success, largely as a result of the growth of the port of Liverpool, but the canal was to bring real benefits to the village of Christleton later.

Railways came in the 19th century and Christleton has a confluence of road, rail and canal, the latter featuring a diversion, still visible today, to enable the tunnelling of the railway under the canal - a unique surviving feature of the history of canals. With the arrival of the motor car in the 20th century adding to these existing transport links and its proximity to both Chester and the rapidly growing cities

of Liverpool and Manchester, Christleton became an attractive residential area for bankers, lawyers and similar professionals working in the cities. They built themselves the substantial Victorian and Edwardian houses that are a feature of the centre of the village today particularly distinctive as they are set side by side with smaller cottages of agricultural origin



The most significant influence on the village centre as seen today came once again from ‘a place of Christians’ in the name of Canon Lionel Garnett, rector from 1868 to 1911, who restored the parish church and built Christleton Grange in Village Road as a new rectory. The High School stands in what was the garden of this rectory.

Arguably it is the original Chester Canal, now part of the Shropshire Union, that survives as the most significant heritage asset for both Christleton and the nation. After World War II, owners of canal hire boat companies based in Christleton recognised the leisure potential of the neglected canal network and their lobbying led to the formation of the Inland Waterways Association. From this point, canals nationwide were revitalised and restored to become the attractive waterfront developments and tourist assets of today. In 2018, the whole length of the original Chester Canal between Chester and Nantwich was designated a Conservation Area.

3. CHRISTLETON AND LITTLETON TODAY

As you read this Neighbourhood Plan it will become clear that Christleton and Littleton are dominated by the Shropshire Union Canal, the A41 and the A51. Traffic cuts through the villages to and from the A41 and A51 alongside children walking and cycling to and from both schools.



These roads handle tens of thousands of vehicles every year, have a poor safety record, suffer twice daily half mile traffic queues and have nitrogen dioxide and PM2.5 levels which are many times the guideline levels, equivalent to a major UK city, and represent a major health risk as measured by CWAC, DEFRA, Imperial College and Parish Council volunteers. As the traffic queues develop on the A41 and A51, the vehicles cut through the centre of Christleton village, doubling the flow of vehicles pushing through the waves of 1600 children and staff arriving and leaving the High School over a period of 25 minutes. This causes complete gridlocks, poor air quality and frequent pavement mountings along Village Road and in front of the school.

The A41 and A51 are major trunk roads connecting Chester to all the surrounding communities and much further afield for example Holyhead and Birmingham. Any suggestion that local buses will remove these queues are just not realistic.

It is therefore crucial to prevent any material increase in traffic volume and maintain the separation of Christleton and Littleton from each other and from Great Boughton, Vicars Cross, Rowton and Waverton



Christleton and Littleton enjoy the distinctive character of housing interspersed with green spaces. The maintenance of these green spaces and the public rights of way are an essential feature of the rural character of both Villages.

The Highways network surrounding and through the centre of Christleton are stretched to breaking point with unacceptable serious and life changing accident injury rates from the traffic and congestion as well as nitrogen dioxide and PM2.5 exposure to residents, visitors, schoolchildren and the elderly as described in the [AIR QUALITY REPORT](#) which results in a wide range of respiratory and cardiovascular chronic conditions.

Air quality remains a huge concern to the residents of Christleton and Littleton and the 1600 High School and 200 Primary School children and staff. NO₂ and PM_{2.5} peak levels are several times higher than World Health Organisation's safe limits. The average levels are double World Health Organisation's safe limits. The EU is projecting to meet their targets by 2030 and reduce exposure to near zero by 2050.

Any large housing development will necessarily bring additional vehicles, as there is no growth in employment locally, and regular use of these vehicles will increase traffic congestion, schoolchildren accident injury rates and make these air quality targets impossible to achieve let alone net zero carbon status by 2050.

PM_{2.5} particulates cause respiratory disease, cardiovascular disease, neurological conditions, cancer, and birth defects. No safe threshold exists. Any exposure causes harm. In our area, they form 60-foot diameter clouds over homes in Great Boughton, Littleton and Christleton where they hover over children's routes to school - they don't dissipate and 20% of strokes are attributable to PM_{2.5} exposure alone.

The Chester Air Quality Management Area covers the city centre. The NO₂ and particulate levels on the A41 and A51 are 80-90% higher than locations inside the AQMA. However, our roads are not included in the AQMA and therefore CWAC is not measuring PM_{2.5} in our area.

NO₂ will decline as everyone buys electric vehicles. However, the smallest particulates - PM_{2.5} won't. They come from brakes and tyres, and will poison our communities for generations to come.

4. VISION

At the start of the Neighbourhood Plan process a Village Consultation - [LINK TO VILLAGE CONSULTATION](#) was sent to all households in the Christleton and Littleton plan area to understand what was important to local residents.

The responses to these initial consultations informed members of the Steering Group in developing the following Vision and Objectives, policies and Parish Council plans for Christleton and Littleton. The Parishes of Christleton and Littleton will continue to maintain their individual characters as vibrant villages with a strong sense of community. They will continue to be areas with a mix of age groups, where local people can live, work and enjoy a high quality of life. Importantly the unique character of each settlement will be retained and enhanced providing opportunities for outdoor recreation and green open spaces rich in wildlife for the benefit of the local communities. Importantly, the Green Belt and strategic green gaps will be retained to provide an important buffer between the settlements and the City of Chester to safeguard the rural character of the plan area.

The Parishes of Christleton and Littleton are sustainable villages that offer two churches, a Primary School, a High School, a shop, a dentist, hairdressers, three pubs, two cafés, a play area, a village pond, cricket club, sports centre and swimming pool that cater for the needs of most residents.



The special character, heritage assets, wildlife and the surrounding countryside must be protected and enhanced making the area an attractive place in which to live, work, play and visit.

This Vision and the following Objectives were published on the Neighbourhood Plan website.

This Neighbourhood Plan is centred on sustainability and in accordance with the Cheshire West and Chester [LOCAL PLAN parts 1 and 2](#). The policies and Parish Council actions in this plan seek to enable development that meets the economic, social and environmental objectives of the two parishes. The presumption in favour of sustainable development, limited by the Highways network, is at the heart of both plan making and decision taking, as set out in the National Planning Policy Framework (NPPF) economic, social and environmental objectives.

This Neighbourhood Plan seeks to reinforce these objectives whilst considering local needs and opportunities across the plan area. The importance of sustainability has informed the process from its initial stages and it is a theme that runs through the whole plan. As a result, the production of the Neighbourhood Plan has provided an opportunity to review and update some of the wider sustainability related issues which underpin the resilience of the communities across the plan area and the quality of the legacy for future generations.

The preparation and production of this Neighbourhood Plan is seen as an opportunity to prioritise and agree proposals and actions the community and Parish Councils can agree upon to enhance the quality of life and empower positive change across the plan area.

5 OBJECTIVES

Christleton and Littleton must

- Maintain and enhance our wildlife corridors, cycling and walking networks and existing green spaces throughout and around the Villages
- Preserve and enhance the many heritage assets, and their setting, in the villages and along the canal
- Encourage young people to remain in the village by supporting affordable, low cost 2/3 bed energy efficient houses that are sympathetic with existing housing and heritage assets
- Reduce the A41/51 traffic, queuing on the A41/51 and cut through traffic through Christleton and Littleton at peak school arrival and leaving times
- Provide secure and sustainable environments for their inhabitants
- Continue to be part of the wider area of Chester but with the distinctive character of the two villages, protected by Green Belt, with local access to open spaces and the surrounding countryside
- Be communities which take pride in their appearance and physical heritage
- Be settlements where people of all ages and incomes can find high quality, well-designed, sustainable housing together with the community services and facilities they need, particularly health and education
- Provide a healthy environment for its communities with access to local and a wider network of open spaces with ample opportunities for cycling, walking, running and riding
- Provide sustainable transport links to both employment and leisure provision on roads, footways and cycleways that are safe for pedestrians, cyclists and people with disabilities
- Preserve locally important views within, into and out of the plan area.
- Be communities that are resilient to the impact of climate change that work towards a 'carbon neutral' sustainable outcome

The general accepted (United Nations) definition of sustainable development is; 'meeting the needs of the present without compromising the ability of future generations to meet their own needs'. The presumption in favour of sustainable development is at the heart of both plan making and decision making, as set out in the National Planning Policy Framework. To meet these objectives the Neighbourhood Plan seeks to enable appropriate development that improves and meets economic, social and environmental objectives for the area.

As well as developing specific planning policies for the area the production of the Neighbourhood Plan has provided an opportunity to review some of the wider sustainability-related issues which underpin the resilience of the community and the quality of the legacy left behind for future generations. Not all of these are directly related to the planning process but have a significant impact upon the health and wellbeing of both communities and therefore limit the housing development that the Villages can accommodate.

To highlight and potentially resolve these issues by 2030 a series of Parish Council actions have been included as part of this Neighbourhood Plan in chapter 7.

6 POLICIES

6.1. HOUSING NEEDS

The table below shows a summary of the overall results to the survey.

Survey Summary	Total Number	% of Households
Total surveys distributed	1,283	100%
Total surveys returned	310	24%
Total needing homes in Christleton and Littleton Parishes	64	5%
Total demand for affordable housing from the survey	12	1%

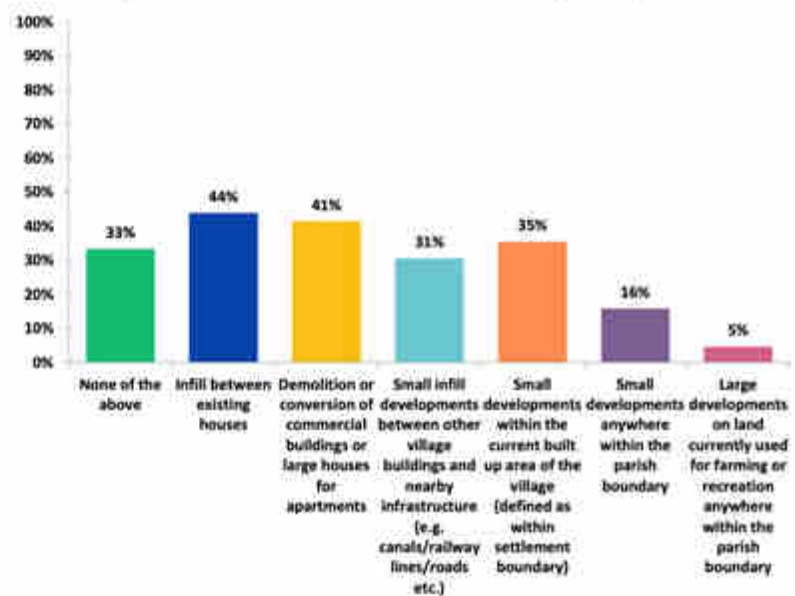
[THE HOUSING NEEDS SURVEY REPORT](#) can be summarised in these charts

Property Type	Affordable housing need		Estimated supply		Shortfall (-) / Surplus (+)
	Housing register need (2023)	Christleton and Littleton Survey – demand for affordable tenures (March 2024)	Pipeline planning permissions – affordable units	Re-lets of existing stock (annual average)	
1 Bed	36	1	4	1	-32
2 Bed	23	7	4	2	-24
3 Bed	13	2	1	<1	-13
4+ Bed	5	2	1	0	-6
Total	77	12	10	4	-75

Q26: Are you in favour of new housing being built in the Christleton and Littleton Green Belt?



Q27: If new housing is to be built in the Christleton and Littleton Green Belt, which of the following locations do you believe are acceptable?



Policy HN1 – Housing need

The housing needs survey identified a need for 12 houses plus 3 applicants on the housing register who have a local connection with Christleton and Littleton.

Policy HN2 – Housing Density

Any new housing **must** reflect the existing local context and **should** provide no more than 16 new houses on any one available and deliverable brownfield or greenfield of less than 1 hectare site immediately adjacent to the village with an average density of 16 dwellings per hectare (dph) to complement the existing villages [HOUSING DENSITY](#) range of 6 to 26 dph. Schemes that exceed the average density of 16dph or the maximum land area of a hectare **should** be refused.

Justification

Christleton is a Local Service Centre and Littleton is countryside. The character of the two Villages is similar and complementary. As there is no growth anticipated in local employment any developments will be car based and not sustainable.

This coupled with the limitations of the current highways network and the accident injury and air pollution health risks to schoolchildren led the Parish Councils to conclude that new housing must be limited to the local needs of residents. New development will be supported in principle provided they are small scale and in character with the developments phased over the period of the Plan. Proposals for new residential development must contribute to achieving well designed attractive and healthy places.

Welsh Water stated for an A41 development – ‘The proposed development site is located in the catchment of a public sewerage system which drains to Chester Wastewater Treatment Works (WwTW). We have considered the impact of foul flows generated by the proposed development and concluded it is unlikely that sufficient capacity exists to accommodate the development within the immediate public sewerage system without causing detriment to the existing services we provide to our customers, or in regard to the protection of the environment.’

Considerations **must** include infrastructure, wildlife, access, air quality, natural environment, heritage assets and their setting. The infrastructure that most limits housing is highways, access, air quality and waste water treatment.

Developments fall into one of these four categories

Policy HN3 – Brownfield or Greenfield Development

Such developments **should** not be co-located with other new housing developments unless there are demonstrable sustainable benefits from doing so. All new development must achieve efficient use of land having regard to the prevailing character of the area and must be the result of a robust design-led approach having regard to the policies contained within this plan and the requirements contained within the Design Code.

Policy HN4– Rural exception sites

Sites of several houses to meet local needs and in character with adjoining developments on land immediately adjacent to the village **should** be approved

Policy HN4 – Infill and backland housing

Infill is described in planning policy as a small gap for up to 1 or 2 houses, sympathetic in style and size to adjacent housing which **must** only be approved for occasional spaces in the Villages **Backland** in our villages generally invades the privacy of existing housing and **should** be refused Christleton and Littleton have an appropriate balance of green space and built area. Infill and backland development will inevitably erode that important balance and will result in an invasion of privacy.

Policy HN5 – Repurposing of empty buildings

There are many examples of old and new properties that are empty. **Priority** will be given to converting these properties to facilitate occupation but subject to the various policies contained in this plan.

6.2 HOUSING DESIGN CODE

The Government have recently published (21.1.26) updated new build design guidance to focus on design quality, creating places of lasting value, that are climate change resilient. This guidance identifies seven features of well-designed places as;

- **Liveability:** Homes close to amenities designed for all stages of life, with communal and private space.
- **Climate:** Buildings that cope with climate change, green spaces, solar panels, green and brown roofs, and EV charging stations.
- **Nature:** hedgehog highways, swift bricks, green spaces and parks, and protection for existing natural features.
- **Movement:** Safe streets, accessible public transport and good parking without car dominated design.
- **Built Form:** Rooms sized to promote health and wellbeing, with good storage, minimising outside noise, privacy and security, sunlight, and good ventilation.
- **Public Space:** well-located spaces that encourage social interaction, with natural surveillance from windows and balconies.
- **Identity:** Character shaped by local history, culture and landscape, reflected in building types and architectural details.

Together with this published design guidance we commissioned a detailed Housing Design Code. In view of the absence of any Borough wide Design Code, and the policies to ensure that developments comply with the existing character of the Villages, the Parish Councils concluded that this Design Code in its entirety must be an integral part of the Neighbourhood Plan.

Policy HDC1 - General Housing Design Code

Development proposals for all new housing in the plan area **must** accord with the required scale of housing development and both the Christleton and Littleton Design Code and the new Design Guidance published by GOV.UK January 2026 ‘to raise the bar for new build development.

This policy seeks to ensure that all new development, together with alterations and extensions to existing buildings in the plan area, demonstrate good design. This means responding to and integrating with local surroundings and landscape context as well as the immediate built environment, and complementing the size, height, scale, mass, rural skyline, layout, access and existing density of the neighbourhood plan area. All new developments including renovations and extensions, must take the opportunities available for improving the local quality and character of

the neighbourhood plan area reflecting the special qualities and distinctiveness of the two principal settlements. This Design Code guide seeks to ensure that all development in the plan area are sensitively located and detailed to make a positive contribution to the character of the plan area and that the landscape character and local distinctiveness is conserved, restored or enhanced

This design guide and the code are practical recipes for Christleton and Littleton. They set out clear rules and guidance to help ensure that the design of new buildings, streets and spaces feel part of the villages and contribute positively to their future. It also offers high level guidance on non-residential buildings and extensions. The code draws on what makes the parishes special, reflects how the villages have grown over time, and how they might continue to grow with care.

The Parishes of Christleton and Littleton are characterised by their rural setting, gentle topography, and distinctive historic pattern of development that features traditional Cheshire vernacular architecture including red sandstone buildings, black and white timber-framed cottages, and Victorian brick dwellings. While Littleton maintains a clear linear arrangement, Christleton has extended from its historic core with post-war development between Village Road and the Shropshire Union Canal, yet still preserves a distinct village feel at its centre.

Both settlements maintain strong rural character through their predominantly two-story detached and semi-detached dwellings (with occasional two-and-a-half and three-story properties) arranged in organic clusters along historic lanes and around village greens, complemented by sports pitches, street trees, and generous front garden planting that creates community spaces while preserving green areas between developed zones.

Properties are thoughtfully positioned to complement rather than dominate the natural landscape, and new developments must respect this established pattern and architectural vocabulary to ensure harmony with the existing built environment—supporting homes and buildings that belong to and are respectful of the existing character of both villages.

How to use this code

The code is structured around key themes: nature, movement, built form, identity, public space, and sustainability in line with the National Model Design Code (NMDC) and the Cheshire West and Borough Council design code (currently in development). Each section includes specific guidance that should be followed to ensure proposals work with, not against, the grain of Christleton and Littleton.

There are three levels of design instruction:

MUST	Mandatory design practices and parameters that must be adhered to. Where policies are designated as a MUST, these are deemed to be essential and non-negotiable. Any proposals that do not meet these requirements will not be approved
SHOULD	Practices and parameters that should be adhered to whenever possible unless there are specific reasons for not doing so through a compliance checklist. This would generally be due to physical constraints. If a should is not adhered to evidence must be presented to justify why not.
Could	Design practices which are recommended to ensure high-quality design is achieved. They will make the granting of planning permission more likely.

However, when creating the Housing Design Code for the Neighbourhood Plan the Steering Group carefully considered the national guidance and decided that most matters included within the Code are mandatory design practices and parameters that **must** be adhered to.

Greenery and Nature

Greenery is integral to the rural feel of Christleton and Littleton. The following guidance will ensure development strengthens the green character of the parishes while providing important recreational spaces for residents and enhancing biodiversity.

Policy HDC2 - Green Infrastructure

Development **must** strengthen existing wildlife corridors by creating green spaces and habitat that connect into the surrounding countryside.

Development **must** preserve important views within, into, and out of the parishes to maintain the existing parkland character of the area (these views are set out in section 6.15 - Local Views).



An illustrative diagram from the National Model Design Code (NMDC) showing (in principle) how green corridors can be designed into developments.

Policy HDC3 - Trees and Planting

Mature trees, hedgerows and ponds **must** be retained and could be used to anchor green spaces. All new development should seek to protect local woodland, trees and hedgerows, wide verges, walls, boundary treatment and existing paving from loss or damage where these contribute to the character and amenity of the plan area.

Tree planting **must** use native tree and plant species that support biodiversity and provide long term resilience. These include wild cherry, bird cherry, hornbeam, rowan, field maple, silver birch and holly for streets and english oak, small-leaved lime, hornbeam, crab apple and wild service tree for public open spaces.

Hedging **must** use hawthorn, blackthorn, hazel, common dogwood, dog rose, elder and guelder rose

One semi - mature tree **must** be planted for each new dwelling.
 Two new semi - mature trees of native species **must** be planted for each tree lost by the development proposed as well as the reconstruction of walls, and paving on the site together with a method statement for the ongoing care and maintenance of the trees, walls and paving.
 All new buildings, structures and hard surfaces **must** be located a sufficient distance away from existing trees within or adjacent to the site to safeguard them and provide for their long-term retention.
 Measures **must** be taken during construction to protect the trees from any damage in accordance with BS5837(or any updated, equivalent standard)
 Rear boundaries facing open countryside **must** include hedgerows or tree planting.



A street tree integrated between on-street parking.(Credit: NMDC)

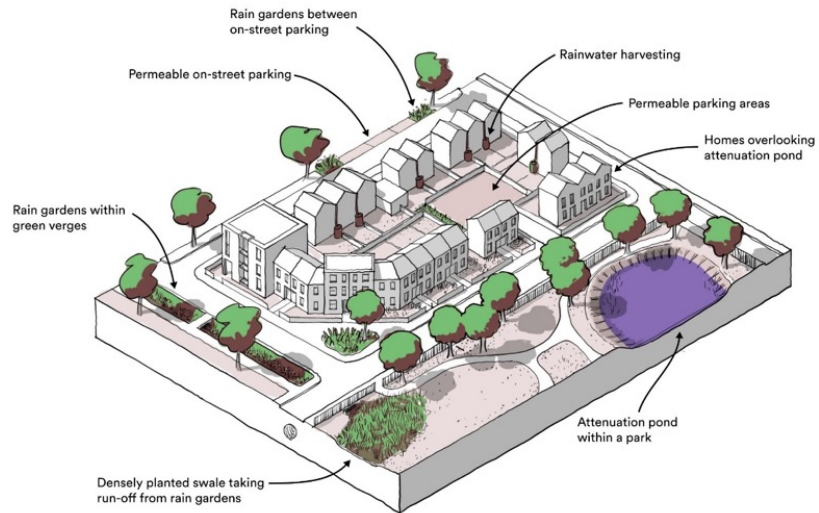
Policy HDC4 - Biodiversity

At least 50% of homes **must** include at least one of the following: bird or bat box, tiles or bee bricks (for locally relevant target species).
 Any boxes/bricks **must** be placed in appropriate locations (e.g. bee bricks on south facing walls and bird boxes at the correct height and orientation) and out of reach of predators.
 Every dwelling **must** be provided with a Swift brick in the wall at least 5m above the ground
 Rear garden fences **must** include hedgehog crossings using a number of products on the market.
 Artificial (plastic) planting of any kind, including artificial grass, **must** not be used within front or back gardens.



Policy HDC5 - Water and drainage

Sustainable urban drainage features (SuDS) such as swales or rain gardens **must** be included in new developments. SuDS **could** also be used to create green corridors.



A diagram showing how sustainable drainage could be integrated into developments (Credit NMDC)



A swale within a green corridor in a development in Alford, East Lindsey, Lincolnshire (Credit: Lincolnshire County Council)

Policy HDC6 - Lighting and dark skies

So as preserve dark skies and protect nocturnal wildlife:
 External lighting **must** use warm-spectrum bulbs (maximum 2700K).
 Bollards and fittings **should** be low-level (below 1.2m), downward-facing and shielded to protect dark skies.
 All lighting **must** minimise light spill to wildlife habitats and open countryside.

Policy HDC7 - Public space

Green public spaces **must** be provided to support social interaction and enhance biodiversity. Communities require clear focal points such as pocket parks, garden squares or informal green spaces that provide a welcoming place to meet, rest and play.

Policy HDC8 - Squares, parks and greens

New developments **must** include squares, parks and greens or other communal gathering spaces that encourage neighbourly interactions and community use. These spaces will strengthen the calm, rural character of Christleton and Littleton.
 New public spaces **must** be well overlooked with building fronting on to them.



Left to right: examples of an informal village green, a formal town square and a formal garden square
(Credit:NMDC)



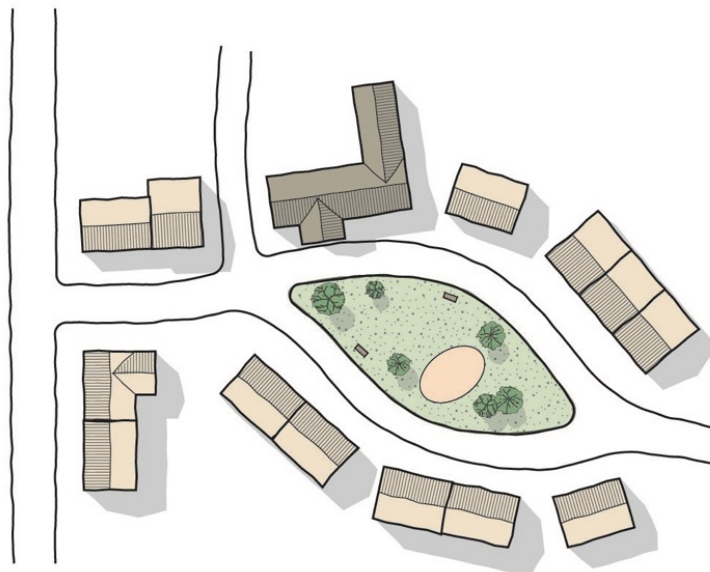
The green Christleton is an example of an informal green that formed at the junction of key routes. Note the irregular layout of buildings around the green give this space an 'open' feel in contrast to the enclosed character of surrounding Streets such as Birch Heath Lane and Village Road.



The green in Christleton is surrounded by a prominent 'features' buildings which frame the space.



New homes overlooking a village green anchored by existing trees, Upton Grange, Chester (Credit: Create Streets)



A small village green within a new development with a play area and well-overlooked by homes (Credit: Create Streets).

Movement

Well-connected, walkable streets are at the heart of a sustainable and sociable village. Development must prioritise walking, cycling and access for all, while reducing the visual and spatial impact of vehicles.

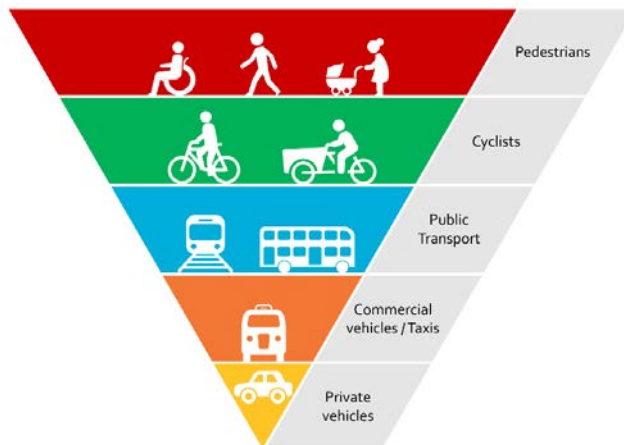
Policy HDC 9 - The street network

Street layouts **must** reflect the scale and grain of a rural village, supporting permeability and safety. Streets **must** be designed according to a hierarchy of users with pedestrian at the top.

The street network **must** be permeable and well-overlooked, linking into surrounding streets and public rights of way.

Cul-de-sacs **must** be avoided unless they include through routes (filtered permeability) for walking or cycling.

Development **must** replicate the organic grain of the village with informal streets of varying widths.



Hierarchy of users (Credit: Create Streets)



Left: a connected street network; Right: a disconnected street network (Credit: NMDC)



An example of a 'cul-de-sac' which provides pedestrian and cycle connectivity.

Policy HDC10 - Walking and cycling routes

All new streets **should** include footways, and developments **should** enable safe, well overlooked walking, wheeling and cycling connectivity to everyday local destinations. New routes **should** be direct, attractive and connected to local amenities. Existing public footpaths **must** be retained and integrated into new layouts with good natural surveillance. If this is not possible, a comparable diversion as close as possible must be provided. Where sites border existing public footpaths and rights of way (such as the Shropshire Union canal towpath) they **must** connect into these routes.



New development in Christleton with direct, well overlooked links to the canal towpath



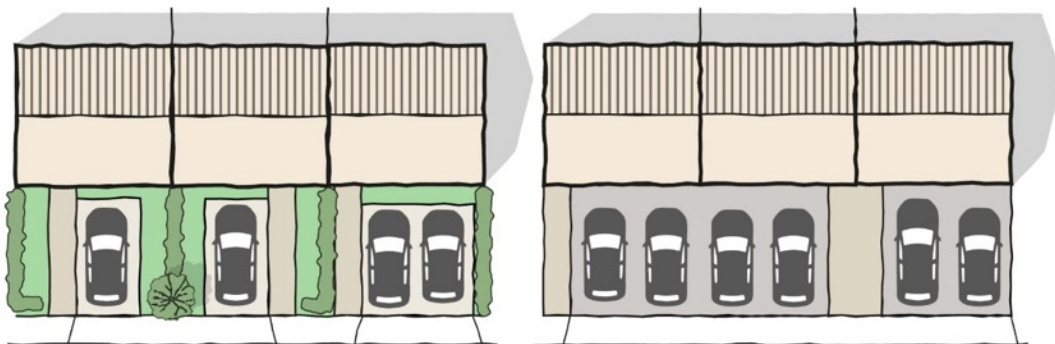
An example of a well overlooked footpath with trees, hedge planting and verge creating a rural feel.
(Credit: Create Streets)

Policy HDC 11 - Car and cycle parking

Parking **should** be positioned to the side or rear of homes to reduce street visual impact. When parking is provided to the front of a building it **must** be softened by hedges or planting. Public cycle parking **should** be secure, convenient and close to building entrances, whether for homes, schools or local facilities. The 2021 Christleton and Huntington Ward 2021 census statistics states that the average car ownership is 1.6 cars per household



An example of in-fill housing with parking to the rear of homes. This creates a street with a stronger sense of enclosure. (Credit: Create Streets)



Acceptable and unacceptable front parking. (Credit: Create Streets)

Policy HDC12 - Garage location and size

Unless impractical, garages **must** be built adjacent to the houses they serve (rather than in front) so that they do not dominate the street scene

To satisfy the requirements for sustainable waste management and active travel, any garage **must** provide a **minimum internal functional area of 3.5m x 7.0m (Single) or 6.0m x 7.0m (Double)**.

These dimensions are required to accommodate modern motor vehicles (including charging infrastructure), a minimum of two bicycles, and three standard residential waste bins, while maintaining a clear 1m unobstructed route for the driver to enter and exit the vehicle comfortably

Garages **must** be designed to reflect the architectural style of the house they serve

Justification

The Plan Area experiences high levels of car ownership, which places significant pressure on existing street scenes. Evidence suggests that the **minimum guidance** set out in the CWaC Parking Standards SPD [CAR PARKING DESIGN](#) often falls short of the practical requirements for local

households. Given the trend toward larger modern vehicles (SUVs) and the necessity for integrated cycle and bin storage, new developments **must** provide garage and driveway dimensions that are **functionally fit for purpose**. This ensures that garages are used for their intended primary function vehicle parking, thereby mitigating overspill parking and protecting highway safety

Policy HDC 12 - Built form

The built form of Christleton and Littleton is characterised by informal street layouts and a uniform scale of homes which are generally two storeys.

There are a range of detached and semi-detached homes with runs of characterful terraces of between one and a half to three stories.

Front gardens with generous planting and gaps between groups of buildings create a distinctive village character.

New development **must** work with this grain, not against it. Rooflines, forms and frontages **must** be designed to sit comfortably alongside neighbouring homes and reinforce the intimate, walkable feel of both villages

The images below rich tapestry of buildings in the village, of varied form and scale, each deeply rooted in the vernacular tradition of Cheshire.



Examples of detached homes many of which are wide fronted.





Examples of semi-detached homes





Examples of terraced homes

Policy HDC 13 - Heights

In keeping with existing built form, new development **should** predominantly be two storeys or two and a half storeys and must not exceed 3 storeys in height to preserve the established low-profile skyline of both parishes.

Three storey buildings **must** be used sparingly (e.g., as a feature building at a key junction or overlooking a village green) and be designed to reflect existing three storey buildings in the villages. They **must** not impact on existing key views.

Rear extensions and garages **should** be subordinate and **must** not block important views.



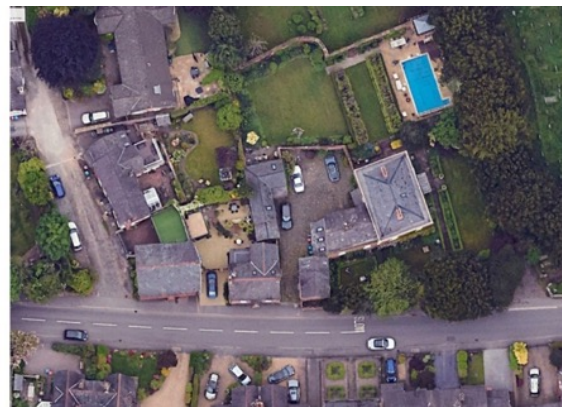
Example of well-designed three storey homes used sparingly to mark a key junction (left) and within a terrace to create a varied roofline (right). (Credit: Create Streets)

Policy HDC 14 - Blocks

Development blocks **should** be designed to create walkable and safe neighbourhoods. Blocks **must** have clear backs and fronts, with back gardens facing other back gardens or parking areas. Back gardens which border streets or public spaces **must** not be permitted. In keeping with the rural character, blocks **should** have an informal structure. Courtyard layouts such as those in the centre of Christleton, **could** be used to create developments with a more rural feel. These must include buildings which front onto the existing street. Blocks **should** be between 50-100m long to create a permeable layout.



Buildings not fronting the street (left) and houses addressing the street (right). (Credit: Create Streets)



Buildings informally arranged around an internal courtyard are present in Christleton and could be a good precedent for new commercial or mixed-use in-fill development that reflects local character.

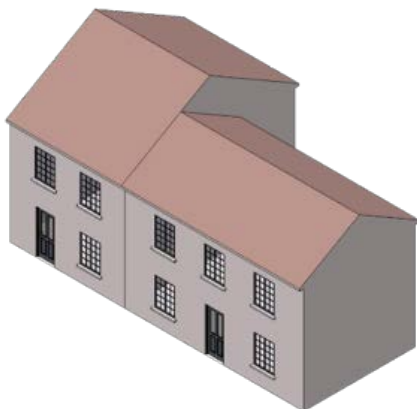


Example of a layout with organic. It is well connected with block lengths between 50m & 100m creating a walkable, permeable layout.

Policy HDC 15 - Building types and forms

Developments **must** have a variety of building types to reflect the historic character of Christleton and Littleton including detached, semi-detached and terraces.

Long, unbroken terraces of more than six homes **must** not be used.



Placing different house types side-by-side create development with an informal, village character. (Credit: Create Streets)

Policy HDC 16 - Minimum space standards

For Christleton and Littleton developers **should** align with the New Design Guidance published 21st of January 2026, adopt the new 'Liveability' and 'Built Form' with Rooms sized to promote health and wellbeing, with good storage. All housing rooms and storage **must** align with the minimum sizes published by the Mayor of London in 2023.

Type of dwelling		Minimum gross internal floor areas (GIA)* and storage (sqm)						Best practice extra space (sqm)		
Number of bedrooms	Number of bedspaces	1-storey dwellings		2-storey dwellings		3-storey dwellings		Built-in storage		
1b	1p	39/37	43/41*					1	1.5	+4
	2p	50	55	58	63			1.5	2	+5
2b	3p	61	67	70	76			2	2.5	+6
	4p	70	77	79	86					+7
3b	4p	74	84	84	94	90	100	2.5	3	+10
	5p	86	97	93	104	99	110			+11
	6p	95	107	102	114	108	120			+12
4b	5p	90	101	97	108	103	114	3	3.5	+11
	6p	99	111	106	118	112	124			+12
	7p	108	121	115	128	121	134			+13
	8p	117	131	124	138	130	144			+14
5b	6p	103	115	110	122	116	128	3.5	4	+12
	7p	112	125	119	132	125	138			+13
	8p	121	135	128	142	134	148			+14
6b	7p	116	129	123	136	129	142	4	4.5	+13
	8p	125	139	132	146	138	152			+14

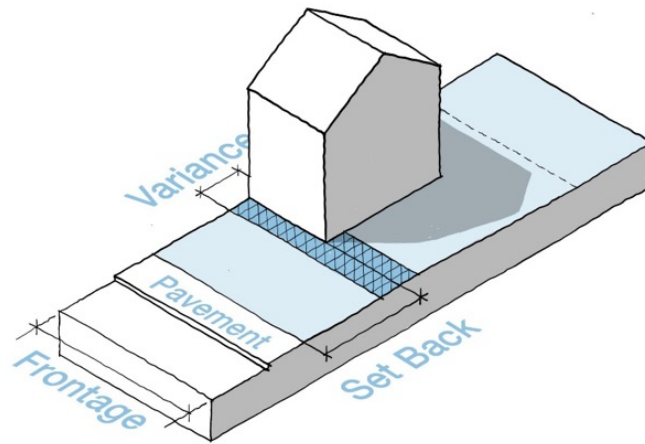
Policy HDC 17 - Building lines

A building line is the alignment of the front face of the buildings in relation to a street or other public space. The nature of this line and its position in relation to the street contribute to the character and identity of a place. To reflect historic grain and built character of Christleton and Littleton development must follow these principles:

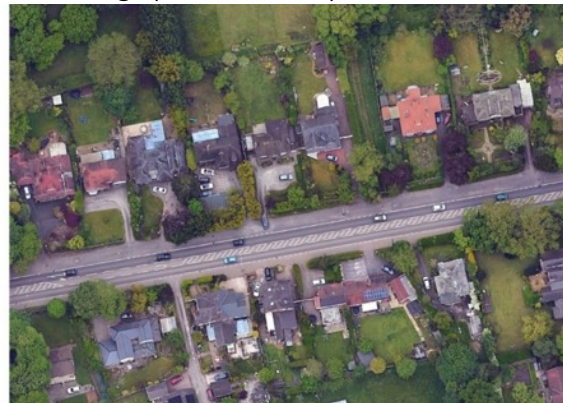
Building frontage **must** be continuous and uniform (buildings facing the same direction)

Building set back **should** be varied (0 to 5m) to create interest and avoid uniformity.

Development **must** reflect the existing building line.



Definition of set back and frontage (Credit- NMDC)



The core of the Christleton has a varied set back while Littleton is more uniform in character.



A small variation in the setback can create an informal character seen in the heart of Christleton.(Credit: Create Streets)



An example of a small infill development in a village centre which continues the existing building line with a rear parking courtyard. (Credit: Create Streets)

Policy HDC 18 - Identity and building façades

The identity of Christleton and Littleton lies in its traditional Cheshire materials, fine detailing and simple but elegant façades.

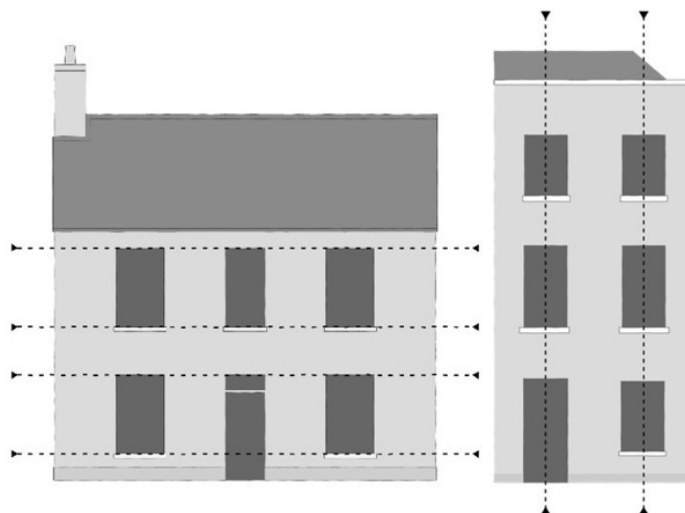
New buildings **must** continue this character, reflecting local historic form by using good proportions, craft and restraint.

A façade is a building's face to the world. It is important not just to the look of a home or business, but to the nature of a settlement. Christleton and Littleton's distinct architectural character, inextricable from its Victorian legacy, adheres to simple principles of coherency, symmetry and order.

Façades **must** be simple and well-proportioned, with symmetry either in the whole or in individual elements.

Window heads and sills **must** align; ground floor windows **should** align with door heads or ornamental elements.

Decorative features such as string courses or cornices **should** be used to add character.



The top or bottom of windows should align and house façades should aim for symmetry either as a whole or within individual elements of the façade. (Credit: Create Streets)



Example of newly built, simple, Cheshire vernacular terraces, Tarporley. (Credit: Create Streets)

Policy HDC 19 - Roofs

Ridge heights **must** vary subtly to create variety and reflect local character.

Roof pitches **must** be between 40–45 degrees to match local vernacular.

Roofs **should** be made of either natural Welsh, Lakes slate or traditional clay ridge tiles

Concrete tiles **must** not be used in Christleton.



The mix of building types and heights creates a varied roofline with an organic feel, Pepper Street, Christleton



Examples of local roof material. Note the detail brick and timber work of the gables.



Examples of local roof materials

Policy HDC 20 - Chimneys

Chimneys **must** be built in brick, sized to suit the building, and reflect traditional local forms. Chimneys **must** be sized proportionally to the size of the building. Larger buildings must have taller or more elaborate chimneys and chimney pots. Chimneys **could** be used to house MVHR ventilation outlets or air source heat pumps.



Examples of local chimneys

Policy HDC 21 - Windows, doors and porches

Windows **must** be timber sash or casement with glazing bars.

Stone or brick window surrounds **must** be used to reinforce character.

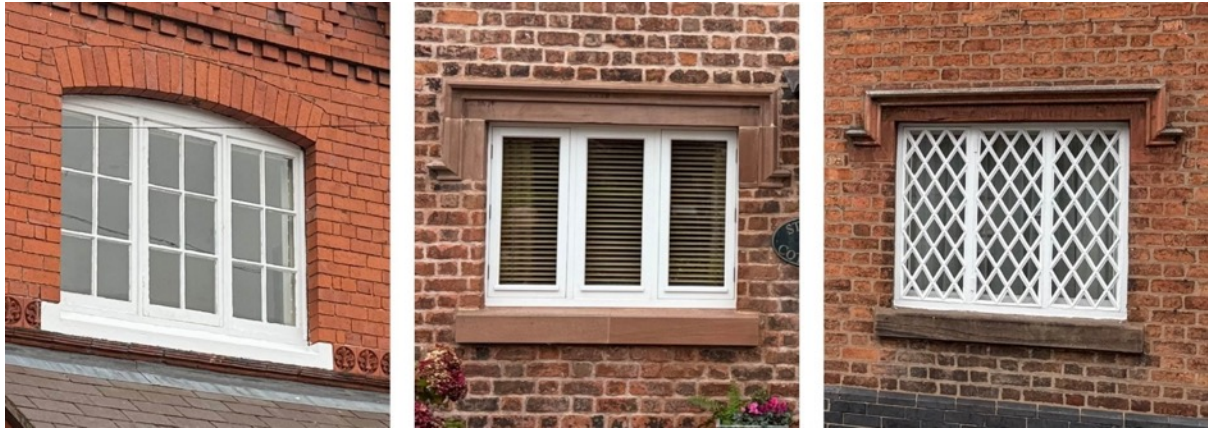
Doors **must** reflect traditional proportions and be framed with high-quality materials and detailing.



Examples of local bay windows



Examples of local sash windows



Example of local casement window. Note the distinctive stone and brick surrounds.

Policy HDC 22 - Materials

The use of good quality, locally appropriate materials is essential for creating new streets and buildings that feel part of Christleton and Littleton. Even the simplest building can be enriched through well-crafted brickwork, windows, and joinery.

The local vernacular is shaped by materials historically available in Cheshire, particularly red sandstone for prominent buildings, red or brown brick and black-and-white timber framing. Many traditional roofs use natural slate, with clay tiles also common in 19th-century buildings. Using these materials, in a way that reflects local detailing, will help ensure new development sits comfortably within the villages' character.

To reinforce the traditional Cheshire vernacular, development in Christleton and Littleton **must** use primary facade materials from the following palette:

Local red sandstone (primary recommendation).

Black and white timber boarding

Traditional brick styles consistent with local area including Red brick, Red/brown brick, Ruabon red brick.

Render or painted brick must be used on no more than 10% of houses.

Brick façades **should** employ traditional brick bonding patterns (Flemish preferred).

Brick walls **must** not use a random assortment of brick colours and tones. Brick walls should use one primary tone of brick.

Bricks **should** be handmade or carefully matched machine-made bricks.



Examples of local bricks



Examples of local bricks

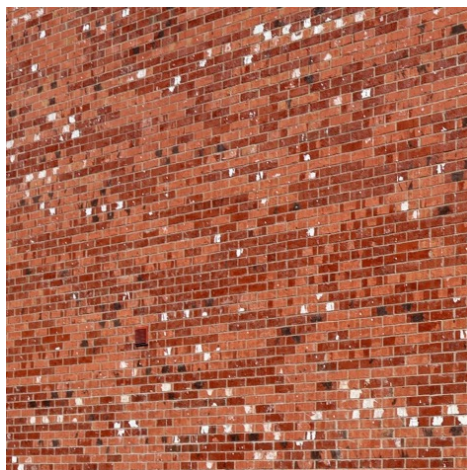


Examples of render and painted brick





Examples of buildings with timber framed detailing, a distinctive feature of the local area.



An example of a wall with an unnecessary and inappropriate variety of tones and artificially distressed bricks

Policy HDC23 - Architectural detailing

Thoughtful detailing gives buildings character and depth and echoes local craft traditions.

Rainwater goods **should** be finished in black cast iron or a visually equivalent material, with traditional bracket profiles.

Natural stone **could** be used for subtle detailing—such as cills, lintels or string courses—where this reflects local precedent and complements the wider materials palette

Cornices **should** be used on rendered buildings to prevent staining.

Policy HDC24 - Boundary walls and fences

Boundary treatments play an important role in setting a new development into its context and help strengthen the existing character.

Front garden boundaries **must** be hedgerows, low stone, cheshire railing or brick walls.

Close-boarded fences **must** not face public areas or streets.



Examples of appropriate boundary treatments.

Policy HDC 25 - Affordable housing

To ensure development in Christleton and Littleton is tenure blind affordable houses:

Must be designed to same external and internal standards as market housing

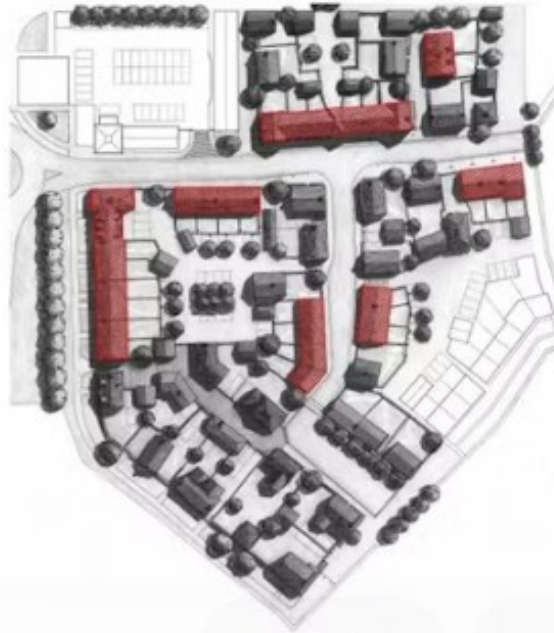
Must be 'pepper potted' throughout a development, not set in front or in one corner



Social housing in Poundbury sits within and throughout the development and is indistinguishable from non-social housing.



An example of poorly integrated affordable home, with a clear differentiation in style and material palette. (Credit: Create Streets)



Affordable housing is dispersed through a development in Poundbury, Dorset (Credit: ESHA Architects)

Policy HDC26 - Extensions

Extensions have a strong impact on the look and character of a building and **should** not be overlooked.

Extensions **should** respect the scale and materials of the original building.

The roof pitch of a multiple storey extension **should** match that of the original building.

Extensions **should** not infringe on the privacy of neighbouring houses.

Policy HDC 27 - Non residential and mixed use buildings

Non-residential and mixed-use buildings **must** be designed to complement the existing character of Christleton and Littleton.

Any new non-residential and mixed-use development **must** reinforce the local vernacular through its scale and use of materials.

Non-residential and mixed use-development **must** be in keeping with the prevailing scale of Christleton and Littleton (generally two storeys with occasional three storey buildings).

Non-residential and mixed-use buildings **must** have active frontages. The exception to this would be industrial or agricultural buildings outside the settlement boundary.

Entrances to community or public buildings **must** be clearly visible from the street and easily accessible.



Example of beautiful vernacular mixed-use buildings (built 1993) in a sensitive historic setting, Tarporley, Cheshire. (Credit: Create Streets)



Light industrial and commercial buildings in a mixed-use development with windows and doors fronting onto the street creating 'active frontage' and a built form which is in keeping with residential form, Tisbury, Wiltshire. (Credit: Create Streets)

Sustainability

Buildings **must** be designed to use less energy, support clean energy and adapt to climate change. Insulation and material choices can reduce environmental impacts of development while enhancing comfort and durability.

Policy HDC28 - Passivhaus standards: advanced environmental design

New buildings and retrofit development in Christleton and Littleton **must** achieve exceptional energy efficiency in line with Passivhaus Standards by adhering to the following requirements:
 Minimum U-values: Walls (0.15 W/m²K), Roof (0.10 W/m²K)
 Triple-glazed windows with warm edge spacers
 Continuous insulation with minimal thermal bridging
 Airtightness target: ≤0.6 air changes per hour at 50 Pa



Examples of triple-glazed casement windows which meet Passivhaus U-values

Policy HDC29 - Air source heat pumps (ASHP) and mechanical systems

Air source heat pumps **must** be designed to the following standards:
 They **must** use mechanical ventilation with a minimum heat recovery (MVHR) efficiency of 75%.
 They **must** use low-energy, quiet operation fans and achieve a maximum noise level of 20 decibels in the bedrooms of adjacent properties when their windows are open.
 They **must** include smart humidity and CO2 sensors.
 They **should** be sited to the rear or side of properties, away from habitable rooms and should not be visible from the street.
 If they are located to the front of homes they **must** be enclosed or screened with planting to minimise visual impact.

Policy HDC30 - Battery storage

Should be located within buildings or in secure, unobtrusive outbuildings.
Must avoid placing battery units in visually prominent or noise-sensitive locations.

Policy HDG31 - Solar photovoltaics (PV)

Buildings **must** integrate solar panels or tiles which sit flush with the roof plane and avoid glare.
 Solar panels **should** not be installed on street facing elevations.
 PV tiles can be used with slate roofs, providing that they blend in.



Appropriate and inappropriate placement of solar panels (Credit: Create Streets)



Example of PV roof tiles

6.3 THE SHROPSHIRE UNION CANAL



Policy SUC1 - The multifunctional role of the canal

The multifunctional nature of the Shropshire Union Canal that runs through the neighbourhood plan area means that the canal, footpaths, greenery and canalside developments **must** continue to provide

- access to the canal for both local residents and visitors to the area for recreational opportunities and physical activity
- a community resource for supporting health and well-being and social interaction, contributing to movement strategies and accessibility and the provision of ambient and safe car-free alternative travel routes for walking/ cycling.
- ecological habitats and biodiversity through contributing to green corridor networks
- a local infrastructure performing multiple functions, such as land drainage, and supporting carbon reduction and environmental sustainability.
- the line of the canal and towpath, the waterside environment, the green setting and its future restoration

Justification

The Shropshire Union Canal supports the health and well-being of the local communities through access to the outdoors and nature, social interaction, and active travel routes as well as sustaining and promoting biodiversity; and helping mitigate climate change. The waterway also provides recreational opportunities and is a tourism asset, well used by leisure boaters and anglers.

The canal corridor is a critical conservation area with a concentration of wildlife, not to be found anywhere else in the two Villages, alongside listed structures which include bridges, locks and adjacent buildings. The canal corridor displays diverse characteristics which must be considered and respected in any development proposals. The setting of the Canal conservation area is an important consideration in preserving its intrinsic quality.

Policy SUC 2 - Development along the canal in Christleton

Development proposals **must**:

- recognise and respect the diverse characteristics of the canal corridor
- acknowledge the localised context of the canal corridor
- preserve the green setting of the canal conservation area which is an important consideration in preserving its intrinsic quality
- maintain a positive connection with the waterway;
- include environmental improvements and make a positive contribution to the visual appearance of the corridor
- incorporate high standards of design, materials, external appearance, layout and landscaping, that respects local landscape character and historic assets and is an appropriate scale for its location
- safeguard the structural and operational integrity of the waterways
- support public access to and recreational use of waterways, as defined in the Local Plan (Part Two) policy DM 37
- retain and enhance existing natural features and make a positive contribution to the borough's ecological network
- avoid any adverse effects on this designated site of ecological importance.

Proposals for the restoration of the Shropshire Union canal and its waterside environment in Christleton **must** be supported where it can be demonstrated that there will be no adverse impact upon the natural or historic environment of the canal corridor, existing wildlife habitats and public access.

Policy SUC 3 -Heritage along the canal in Christleton

Any development **must** safeguard the heritage value of the local Canal Conservation Areas and Listed Structures, and protect the historic significance and setting of the canal corridor.

The canal falls within the Chester Canal Conservation Area and Christleton Conservation Area and contains the Grade ii listed Greenfield Lock 8 on the canal. In addition, the Canal Trust own and manage three Grade ii listed bridges along this stretch, all of which are contemporary to the canal.

Bridge 120 - Rowton

Bridge 121 – Quarry

Bridge 122 – Christleton

The historic environment of the canal network contributes to the character, heritage value and cultural identity throughout Christleton and Littleton and therefore the Canal network must be reflected appropriately in any Heritage policy



Policy SUC 4 - Biodiversity buffer

Any developments alongside the canal **must** be laid out with gardens facing the canal and a **20m biodiversity buffer** between any property perimeter and the Canal River Trust boundary. The local context set-back needs **must** be large enough to allow for structural landscaping in the form of specimen trees and hedgerows which would further enhance bio-diversity and screen new development from the canal corridor.

In line with national policy requirements, development **must** also deliver a Biodiversity Net Gain of 10% ,within the Neighbourhood Plan area , which means a development will result in more or better-quality natural habitat than there was before development. Planning permissions, with only rare exceptions, are required to deliver this Net Gain of 10% which is calculated using a Defra Biodiversity Metric (pre and post development value) and habitats will need to be secured and maintained for 30 years.

The Statutory Biodiversity Metric requires that planning applications that include land within the site boundary that is within 10m of a canal or river are supported by an assessment of the baseline condition of the watercourse. Unless exemptions apply, in these circumstances, developers **must** deliver a minimum 10% net gain in watercourse biodiversity units. DM44 (Part 1) (Para 16.13) outlines the importance of protecting and enhancing the natural environment through identifying ecological resource, calculating how to deliver biodiversity net gain after development and of identifying ecological networks to link up areas of habitat.

Justification - Wildlife

The **Shropshire Union Canal**, surrounding fields and farmland through Christleton, Waverton and Rowton villages, provide a rich and valuable habit for a wide range of wildlife species which **must** be conserved. David Cummings BEM has been observing, recording and photographing wildlife here for 55 years. The following species have been observed by David in the last two years, along the canal or within 20m of the water's edge.

Wildfowl; Mute swan*, Grey Heron, Cormorant, Moorhen*, Mallard*, Tufted Duck, Kingfisher* (2 territories) Icelandic Pink Feet, Greylag and Canada Geese.

Birds of Prey; Kestrel*, Buzzard*, Sparrowhawk*, Red Kite, Barn* and Tawny* Owls.

Other farmland/woodland species; Magpie*, Jackdaw*, Jay*, Crow*, Rook* and Raven. Pheasant and Quail.

Hedgerow and Garden birds; Chiff Chaff*, Willow Warbler, Black Cap*, Robin*, Wren*, Great*, blue*, Coal* and Long Tailed Tits*. Starling*, Hedge* and House Sparrows*, Chaffinch*, Greenfinch, Siskin and Redpoll. Swift, Barn Swallow* and House Martin*. Skylark & Meadow Pipit, Redwing, Fieldfare and Blackbird*, Mistle* and Song Thrush*.

***indicates breeding species**

Insects & butterflies; Brown and Southern Hawker Dragonflies, Common Blue and Banded Agrion Damselflies, Common Darter, Red Admiral, Peacock, Brimstone, Small Tortoiseshell, Comma, Meadow Brown, Speckled Wood, Orange Tip, Gatekeeper, Ringlet, Small Copper, Small Heath, Painted Lady, Common and Holly Blue, Large, Small and Green Veined White,

Bats; Horseshoe and Pipistrelle.

Mammals; Mole, Badger, Fox, Otter, Mink, Roe Deer, Brown Hare, Hedgehog, Frogs and Toads.

Flora; Cuckoo Flower, Marsh Marigold, Daisy, Burr Reed, Red and Common Valerian, Meadowsweet, Buttercup, Coltsfoot, Snowdrops, Bluebells, Ramsons, Aram Liley, Pink Campion, Greater Stitchwort, Birds Foot Trefoil, Wood Sorrel, Herb Robert, Kidney Vetch, Cow Parsley etc.

Policy SUC 5 - Water quality and contamination

New development proposals along the canal in Christleton **must** safeguard water quality and protect against potential contamination of waterways, during construction and operation.

Policy SUC 6 - Green Infrastructure

The Shropshire Union canal that runs through the neighbourhood plan area makes a crucial contribution to the green and blue infrastructure in the plan area that **must** be conserved.

Justification

The definition provided for green Infrastructure in the NPPF (December 24) Glossary) refers to blue spaces, which includes the canal waterway network. 'A network of multi-functional green and blue spaces and other natural features, urban and rural, which is capable of delivering a wide range of environmental, economic, health and wellbeing benefits for nature, climate, local and wider communities and prosperity.'

In the plan area the Shropshire Union canal contributes to the provision of Green Infrastructure which enhances the landscape character, amenity, recreation, flood mitigation, water quality, sustainable transport, Local Transport Plan priorities, cycling strategy, resident health and welfare, biodiversity, geological and historical conservation, ecosystem services, tourism and the economic and regeneration of the plan area and the borough.



Policy SUC7 - New Moorings

Proposals for new mooring facilities within the neighbourhood plan area will be supported where:

- the development would not result in the obstruction of the waterway for other users
- the proposal would not result in the degradation of the canal environment
- the proposal is appropriately located and designed
- built development is of an appropriate scale and ancillary to the mooring facilities;
- the proposal does not compromise the green setting and wildlife of the canal

Justification

These policy provisions will enhance the Shropshire Union Canal environment, protect wildlife, safeguard the structural and operational integrity of the waterways, retain mooring facilities, support public access and recreational use of waterways as defined in Local Plan (Part Two) policy DM 37.

In line with the above policies, development **should** enhance permeability and help create legible pedestrian and cycle friendly environments. Canal towpaths offer a sustainable, traffic-free route for walkers and cyclists, reducing reliance on private motor cars to access services and facilities and also encouraging healthier lifestyles. As such, canals and towpaths can contribute significantly towards encouraging active travel, supporting more sustainable modes of transport and providing a valuable recreational resource that is free-to-use for local communities.

6.4 HEALTH AND WELL BEING

Policy HWB 1 - Health and well-being

In order to meet the health and well-being needs of local residents' proposals **must** be supported that:

- provide new or improved health facilities across the plan area
- support improved links to healthcare in the plan area
- promote safe and accessible environments and developments with good access by walking, and cycling
- consider the specific requirements of different groups in the community (e.g. families with children, older people, people with disabilities, service families) in all relevant development
- promote high quality greenspace, and access to this across the plan area

Development that gives rise to significant adverse impacts on health and quality of life (e.g. soil, noise, water, air pollution, light pollution, land instability, etc) including residential amenity, **will not be supported**.

Justification

To encourage developments that maximise opportunities for activities enhancing recreation, sport, leisure, health and well-being both now and in the future

6.5 GREEN GAPS

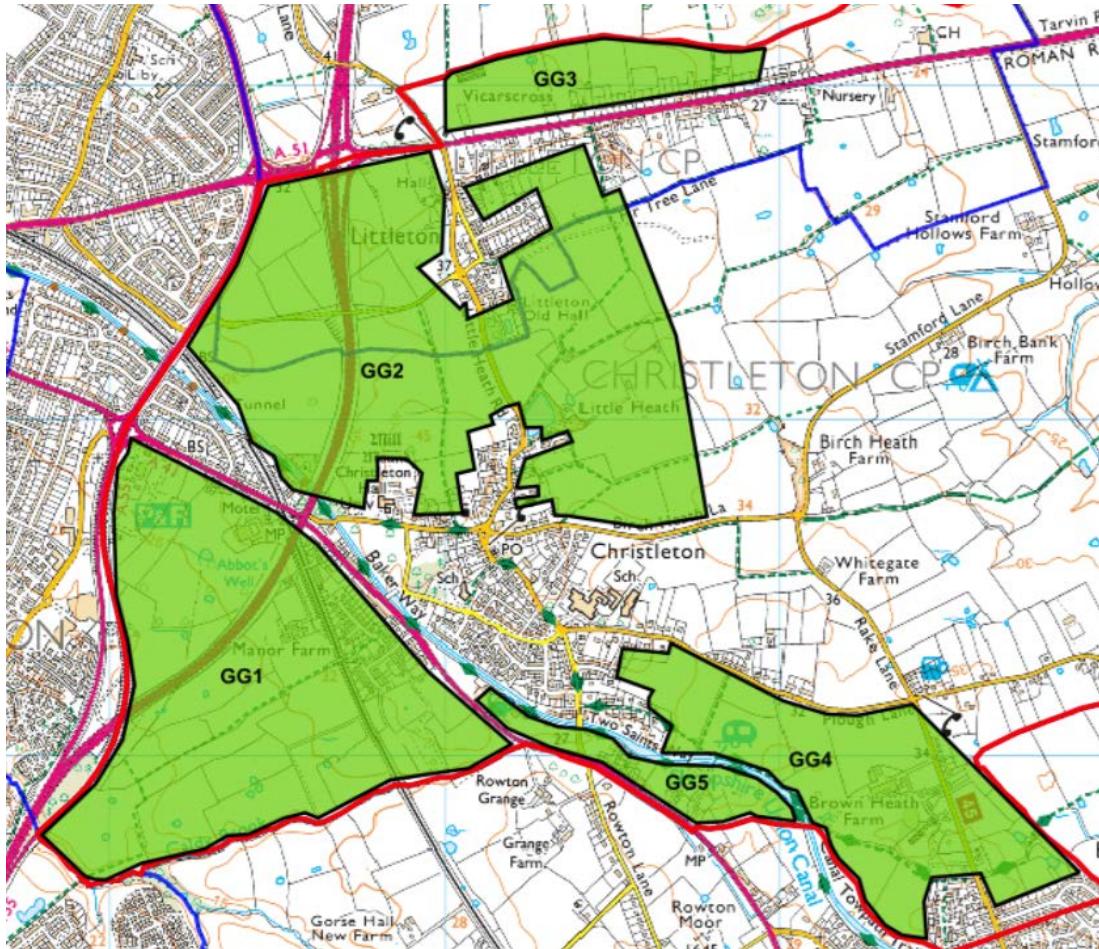
Policies GG1-5 - Green Gaps

Development **must** not take place in a Green Gap unless very special circumstances can be demonstrated or where development supports the role and function of the GREEN GAP.

The Neighbourhood Plan designates sites **within the plan area** as Green Gaps (GG) that separate communities from each other. They have a specific function and add significantly to the amenities of the area. The sites that meet the GG criteria are identified in the map and policy list below.

The green gaps **prevent housing developments**, reinforce the protection provided by greenbelt, prevent settlements from merging into one another and provide access to open countryside. Failure to maintain these green gaps will:

- erode the physical gap required to maintain the integrity of each settlement
- adversely affect the visual character of the landscape
- significantly affect the undeveloped character of the green gaps
- lead to coalescence of the settlements
- eliminate the exercise and health benefits of easy access to open countryside



Justification

Policy GG1 - Land separating Christleton from Great Boughton and Huntington

- provides access to open countryside for all three Parishes
- provides Public Rights of Way (PROW) access between the Parishes
- Christleton and Littleton do not need the large number of proposed houses possible on GG1
- GG1 checks the unrestricted sprawl of large built-up areas and preserves the setting and special character of Chester – proposed developments in GG1 are therefore not grey belt
- the single point access points offered by any development on and off an already capacity limited A41 trunk road south in the midst the main cycling and walking route to the schools and complex vehicular movements on and off any developments, adjacent properties and adjacent roads and junctions is completely unacceptable. Peak A41 vehicular flows will coincide with stationary traffic north, fast moving traffic south and waves of children cycling and walking to and from the High School and Primary School. Any proposed developments will increase an already unacceptable serious injury risk to schoolchildren.
- Christleton and Littleton Parish Councils have an Air Quality Working Group. Their latest report shows that CWaC fixed monitor measurements exceed the WHO limit for NO₂ and the handheld plume measurements show that the NO₂ and PM_{2.5} are many times the WHO limits at A41 peak times. Further traffic on the A41 will make it impossible to meet the earlier DEFRA targets just announced.
- the Battle of Rowton Heath was centred on an area bounded by the Shropshire Union Canal and Rowton Lane but the wider skirmishes occurred over this and a wider area

Manor Farm site in GG1

- a new High School located in GG1 was offered as a planning inducement, to replace the High School in the middle of the Village
- children from Huntington, Rowton and Great Boughton cross the A41 from west to east with the High School and Primary School on the east side of the A41
- if the High School were moved to the west side of the A41 many more children from Christleton, Littleton, Barrow, Waverton and other communities around Chester would have to cross the A41 from east to west with unacceptable accident injury and air quality risks
- the peaks flow of pedestrians, cyclists, cars and buses accessing the development and the proposed school would cause the A41, from the Hamburger junction (at the north end of the A41) to Waverton, to completely gridlock for longer periods every morning and evening leaving commuters, HGVs, school staff and pupils unable to get to work or school on time.
- the developer has suggested a Hamburger junction bypass using White Lane and a road past a new housing development. This is fraught with risks and will not avoid the A41 queueing and gridlocks
- the Christleton Parish Council have taken legal advice and the planning inducement of a new school breaches planning law and was not supported by the Parish Council and the High School at the time
- The Battle of Rowton Heath was centred on an area bounded by the Shropshire Union Canal and Rowton Lane but the wider skirmishes occurred over this and a wider area which provides a crucial rural setting for the Battle



Land behind the Mercure Hotel in GG1

- as with the Manor Farm development the A41 is already gridlocked with traffic queues from Waverton to the Hamburger junction on many occasions during the day but particularly during peak times in the morning and afternoon.
- a detailed transport assessment has concluded that access directly on and off a large proposed housing development, across stationary and fast-moving traffic, close to the junction with Toll Bar Road with a history of serious injuries is unacceptable



Policy GG2 - Land separating Christleton and Littleton and Vicars Cross

- provides open access to open countryside for all three Parishes
- provides a critical piece of green belt that separates Littleton from Christleton
- GG2 checks the unrestricted sprawl of a large built-up area and preserves the setting and special character of Chester – it is therefore not grey belt
- very important PROW walking routes and distant views of the Villages
- Christleton does not need the large number of houses possible on this site
- long rural view would be irreversibly damaged by housing
- a small development proposal on the Tarvin Rd at the extreme edge of GG2 is too densely packed, room and garage sizes are too small, biodiversity is damaged and the houses do not comply with the housing design code



Policy GG3 - Land separating Littleton from Guilden Sutton

- provides open access to open countryside and PROW access for both Parishes
- GG3 checks the unrestricted sprawl of a large built-up area – proposed developments in GG3 are therefore not grey belt
- if Guilden Sutton develop a neighbourhood plan they will create an adjoining green gap

Policy GG4 - Land separating Christleton from Waverton

- provides open access to open countryside for both Parishes
- critical piece of green belt that separates Christleton from Waverton
- GG4 checks the unrestricted sprawl of large built-up areas – proposed developments in GG4 are therefore not grey belt
- very important PROW walking route and distant views of the Village
- Christleton does not need the large number of houses possible on this site

- long rural view would be irreversibly damaged
- a planning application for a development on the boundary between Christleton and Waverton is in low lying land, subject to flooding, requires a sump and pump for all waste water. The development is at the extreme south end of this green gap and does not comply with the Housing Design code



Policy GG5 - Land separating Christleton from Rowton

- provides open access to open countryside for both Parishes
- critical piece of green belt that separates Christleton from Waverton and Rowton
- GG5 therefore checks the unrestricted sprawl of large built-up areas, preserves the setting of Chester and the uniquely historical civil war site of the Battle of Rowton Moor
- GG5 is therefore a green gap and not grey belt
- very important PROW walking routes and distant views of the Village
- Christleton does not need the large number of houses possible in this area
- long rural view would be irreversibly damaged
- a planning application submitted for a development between the Cheshire Cat and Rowton Bridge Road would compromise separation from Rowton and the site of the uniquely historical civil war Battle of Rowton Moor
- this is one such example has inadequate access off narrow Rowton Bridge Road and traffic queueing from the A41 to Rowton bridge

6.6 LOCAL GREEN SPACES

Policies LGS 1-11 - Local Green Spaces

Development in Local Green Spaces will only be supported in very special circumstances and **must** not be permitted unless it can be demonstrated that the harm to the Local Green Space is outweighed by other material considerations.

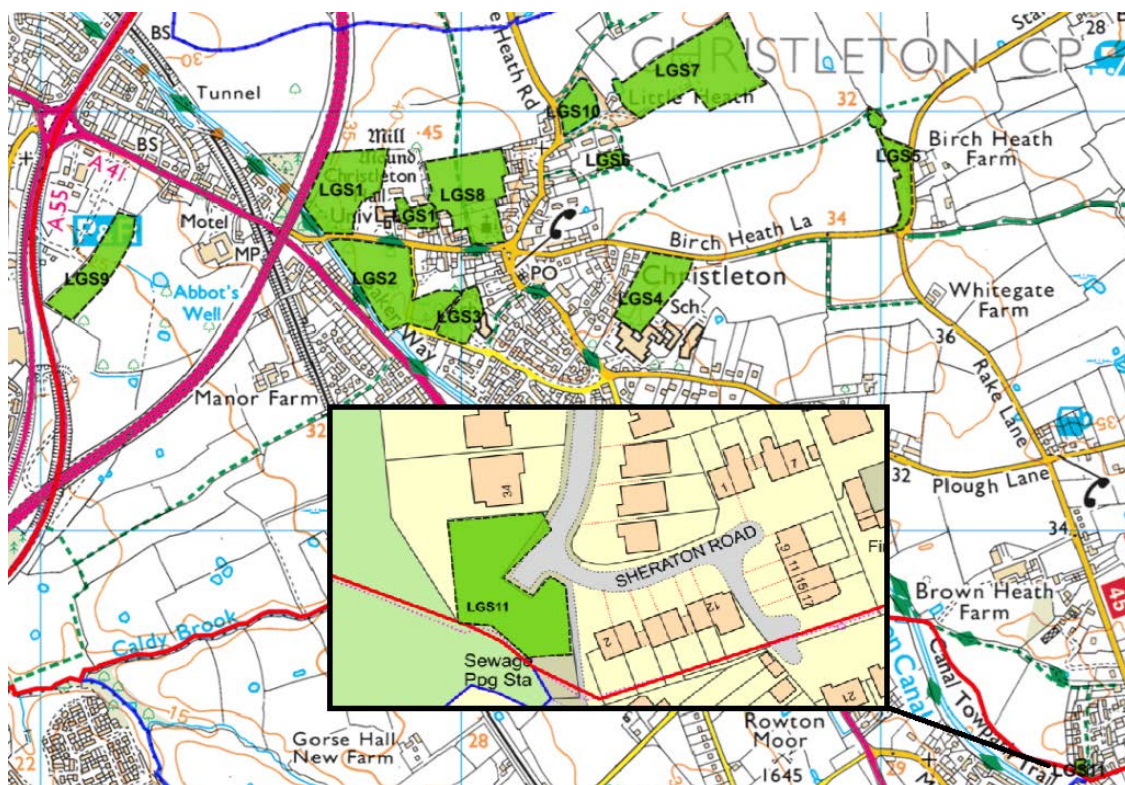
All of the green spaces on the map and listed below have been assessed against the criteria set out in **paragraph 106-108** of the **2024 NPPF**. Green spaces **must**

- be close to the community it serves
- preserve the openness and character of the space
- maintain the landscape and visual amenity of the area
- be demonstrably special to a local community because of its beauty, historic significance, green setting for heritage assets, recreation value (playing field), tranquillity or richness in wildlife
- be local in character and not an extensive tract of land.

Justification

Local Green Spaces are designated for special protection due to their particular importance to the local community. These include the green settings for heritage and conservation assets, regular activities and recreational use. **Paragraphs 106-108** of the Revised National Planning Policy Framework **2024** give Neighbourhood Plans the ability to designate important areas of open and green space as Local Green Space. Small scale development that support the use of the green space such as seating, paths, play equipment, recreation, habitat improvement or community facilities may be permitted where it is ancillary to the function of the space and does not undermine its openness

The Neighbourhood Plan designates sites **within the plan area** as Local Green Spaces (LGS). They have a specific function and add significantly to the amenities of the area. The local green spaces that meet the LGS criteria are identified in the map and policy list below and are justified against the above NPPF criteria



Justification

Local green spaces serve a number of critical roles

- part of the public realm where informal social interaction can take place
- provision of space for sports and physical activity
- valuable habitats for wildlife and natural corridor and spaces between the built areas
- network of paths and spaces enabling movement through and between communities
- providing an attractive setting for residential properties
- forming part of the character and green setting of historic areas and buildings

The following local green spaces contribute to recreation, setting and quality of place which together with the history, heritage, historical buildings, canal side amenity and public realm are enjoyed by local residents and are an integral part of the attraction of the area

Policy LGS1 - Law College playing field and orchard

Green field designated a playing field only 100m from the Village Green

Separates the new Law College housing development from the unacceptable noisy A55

Large tree planting around the edges of the field which link to the canal and nature connectivity

The walled orchard is part of the grade ii listing and a crucial setting for Christleton Hall.

The housing developer agreed to retain the rare apple trees in the Orchard and provide the residents of the new houses with access to the playing field and the orchard. The housing development builder felled all the trees 'by mistake' - this green space **must** be reinstated as an orchard.

Access to the playing field and the orchard **must** be provided to the residents of the new houses.



Policy LGS2 - Cullimore's field

Green space less than 100m from the Village Green land bounded by Pepper St, Quarry Lane and the Primary school playing field that separates Christleton from the heavily used A41 and provides a unique grouping of trees in the middle of the Christleton Village, open green space and a critical setting for the Shropshire Union Canal and the Trooper hump backed bridge and a picturesque approach to the Village off the heavily urban A41. The field has also been used as a location for bikers meets and special events.



Policy LGS3 - Primary school playing field

School playing field less than 50m from the Village Green and bounded by trees, gardens and the school. Critical space in the centre of the Village used by the Primary School and used for village events such as the annual village fete and monthly markets. Intimately surrounded by Village housing where the delightful cries of schoolchildren can be heard during term time.



Policy LGS4 - Part of High School playing field

Rectangular piece of land 100m from the Village Green and bounded by the The Park, Birch Heath Lane and the High School which separates the school from the centre of the Village. Critical High school playing field which separates Christleton Village housing from the school buildings



Policy LGS5 - Birch Heath Common

Small woodland to the East of the Village and 200m from the Village Green surrounded by fields with a small pond in the centre and profusion of wildflowers and several Hazel trees. The area provides nesting sites, water sources and food for birds, insects, and small mammals. It is valued by the village as a good walking and wildlife location and provides interconnectivity for nature
A beautiful rural walk, past a pond which connects footpaths.



Policy LGS6 - Legion meadow

A lowland wildflower meadow between the Pit and Little Heath Road, and only 50m from the Village Green, connecting footpaths in the Village centre , supporting a range of wildflower species (such as yellow rattle and purple orchid, insects, bees and butterflies (Emperor dragon flies have been spotted here).



Policy LGS7 - King George V recreation area

Close to the Village centre to make for easy access

Play area, playing field and cricket pitches

Activities for everyone on the edge of the Village that feels like open countryside

Valued open recreational green space with mature tree planting and surrounding fields. An additional 16 native standard trees have just been planted providing habitats, flooding, and pollution control.

Additional biodiversity enhancement is planned to include wildflower meadow planting, native bulb planting, and habitat creation for bats, birds and insects.



Policy LGS8 – St James Church Graveyard

Essential secluded end of life provision for our dearly departed

St James Church yard is an area of natural beauty that has been working on re-wilding sections of the churchyard to help offset its carbon footprint and achieve net zero. Butterflies and pollinators have benefited from this.



Policy LGS9 – Boughton Heath allotments

On the edge of the Village and bounded by heavily used main trunk roads
A lesson in independent healthy living



Policy LGS10 – The Pit

The large pond on the edge of the village, only 100m from the Village Green, which is valued as an amenity, heritage, wildlife habitat, and resource for wildlife including fish, aquatic birds, insects and plant species. A crucial amenity for young and old alike



Policy LGS11 – Capesthorne Community Orchard

The Orchard is 400m from the Village Green behind a housing estate and backing on to fields
The Orchard has just been enhanced with two new trees and now has a total of 10 trees, which provide a source of pollen for pollinators, food for wildlife and tranquil access for residents

6.7 INCIDENTAL OPEN SPACES

Policies IOS 1-3 - Incidental Open Spaces

In addition to the above Local Green Spaces there are three important incidental open spaces (IOS) shown on the map and listed below which provide a green and open setting to the dense residential and heritage assets in the centre of the Village. They do not fall under the NPPF criteria but add to the open and rural appearance of the villages and **must** not to be used for development



Justification

Policy IOS1 - Village Green

A setting for the Church, Parish Hall and numerous heritage buildings set alongside a Dentist, Care Home, General Store, Pub and Hairdressers





Policy IOS2 - Wide green verges on Village Road and Quarry Lane

These green verges provide a rural setting for a critical road in the Village Centre Bounded by housing, the High School, the Ring O'Bells and the Village Green

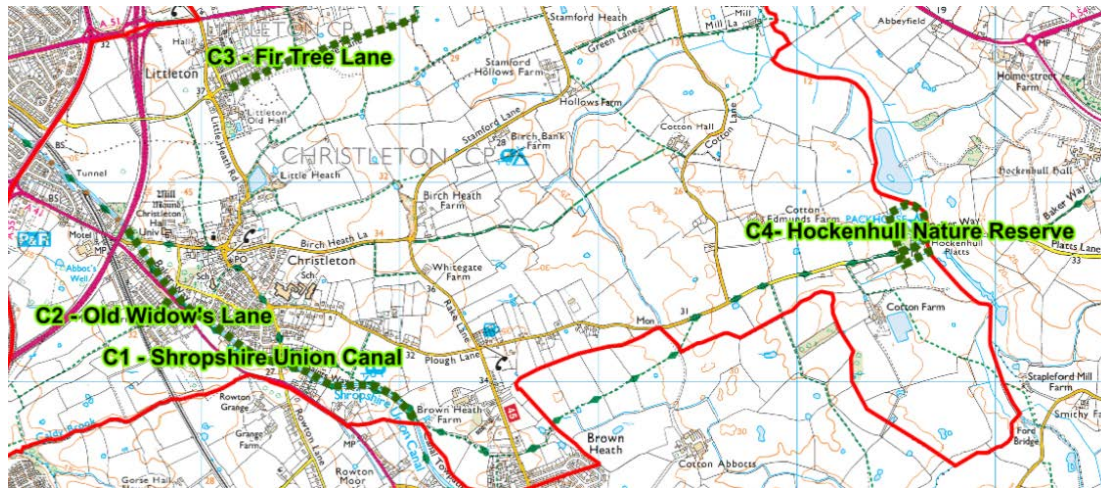
Policy IOS3 – The Daffodil field next to the Methodist Hall



6.8 CONSERVATION CORRIDORS AND WILDLIFE RESERVE

Policies C1-4 - Conservation corridors and Wildlife reserve

There **must** be no housing development or no access to housing development in the critical wildlife areas shown in the map and listed below



Justification

Policy C1 – Shropshire Union Canal

The Shropshire Union Canal is a critical conservation corridor which passes through part of the plan area. It has been designated as a conservation area from Ellesmere port to Nantwich, a total length of 27 miles. It is a great benefit to wildlife providing habitats and resources for a range of species, such as mammals, fish, and birds. Including Herons, Kingfishers, Otters, Dragonflies, Demoiselle, Sand Martins and Dippers. As well as a range of flora and fauna. The Canal River Trust have provided a set of general development, heritage, ecology, water quality, health and well-being, green infrastructure and mooring policies to protect the Shropshire Union Canal. The multifunctional role of the canal has been reviewed in detail in section 6.3.



Policy C2 - Old Woman's Lane

This is a canopy covered walkway and conservation corridor and old carriageway, rich in wildlife, with mature trees on either side of the lane and a walk through to Manor Farm with open views to the mountains of Wales. Barn Owls use this corridor for nesting and hunting.



Policy C3 - Fir Tree Lane

Is almost certainly a unique Roman and medieval route between settlements.

Fir Tree Lane was a route for workers to brickworks, quarries, sand extraction and a candle factory on the Tarvin Road

A canopy covered bridle path with an abundance of hedges, trees and a variety of habitats over a short distance valued for its nature, interconnectivity and links to the footpath network.



Policy C4 - Hockenhull Platts Nature Reserve

Hockenhull Platts nature reserve sits by the River Gowey and is a special place to enjoy a short stroll along the Mill Trail and over the quaint Roman bridges that traverse the reserve. The reserve has several habitats, including ponds, reed beds, mature hedgerows, woodland and species rich wet meadows. Many mammals are glimpsed here including foxes, otters and water vole. The reserve is rich in bird life with over 70 species recorded including summer sedge warblers, grasshopper warblers and reed warblers. Otters have been recorded along the banks of the river. The reserve also has snipe over winter and good numbers of dragonflies and damselflies in the summer months. The reserve is a common nesting and hunting site for barn owls who have been spotted near the village and it is assumed that they use areas of the village as a flight path and for hunting.



6.9 HERITAGE ASSETS

The villages of Christleton and Littleton are rural locations with settlements that were originally agricultural but now largely residential communities. The village green forms part of the nucleus of Christleton, the grain of which generally conforms to the sequence of linear roads, whilst the core centres around St James' Church and the village green. Christleton is mentioned in the Domesday Book, and the tower of the church of St James dates from the late 15th century, whilst Christleton Old Hall was constructed in 1605, and the Manor House of late 16th century origins.

Around the Village Green are a series of Grade ii listed buildings, including;

- Church of St James and associated with the church, the war memorial headstone within the burial ground, sundial, and lych gates



- The Manor House
- Old Hall alongside its gatepiers and walls, bread ovens and memorial to a dog in the rear garden
- The memorial shelter on the green itself – the Pump House
- Rock House and shop
- Corner House and Sanden Cottage
- K6 Telephone kiosk
- Primrose, Holly, Clematis and Rose Cottage
- The Old Surgery

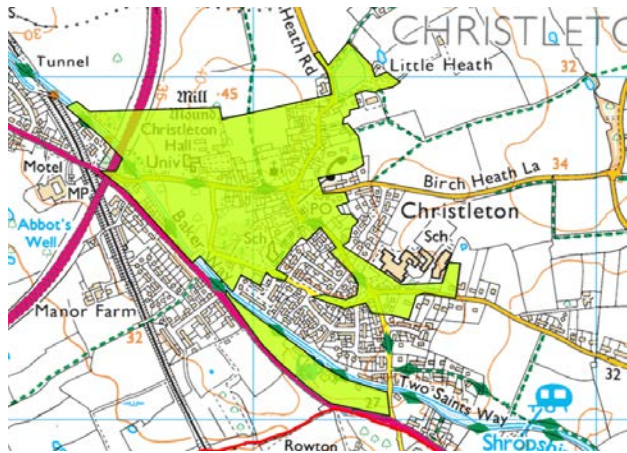
The following plans, documents and strategic policies support policies HE1 and HE2:

National Planning Policy Framework

Cheshire West and Chester Local Plan Strategy Parts 1 & 2

Historic England Neighbourhood Planning and the Historic Environment Advice Note No.11

The [CHRISTLETON HERITAGE ASSESSMENT](#) and [CHRISTLETON CORONATION TRAIL](#) are important reference documents



Policy HE1 - Christleton conservation area

All new development within the Conservation Areas **must** comply with the following design principles:

- new development should respect its context in terms of scale, height and massing
- the green setting of the conservation area is a crucial aspect of its special character and therefore - development proposals within the setting will require an impact assessment
- facades should normally conform to the original building line
- materials should blend with or complement the brick, slate and stone used in the area
- roofs will normally be pitched
- the Housing Design Code outlined in this plan

Justification

The Neighbourhood Plan area contains the Christleton Conservation Area. It is likely that a number of developments in the plan area will take place inside, adjacent to, or have some impact on a conservation area. The community highly values this area and has called for measures that **must** be applied to protect and enhance it more effectively.

This policy seeks to achieve this through setting out general design principles with which all new development should comply, including householder extensions. This policy is based on the criteria for development within each conservation area appraisal.

Policy HE 2 - Re-use of historic buildings

Planning applications which propose the re-use of historic buildings (designated and non-designated heritage assets) **must** be supported where they contribute to their long-term use and safeguard the significance of the heritage asset.

Proposals will be considered against the following criteria where applicable:

- that the external alterations are minimal and do not significantly alter the appearance of the building
- the internal fabric of the building should be preserved where possible
- high quality materials, fixtures and fittings will be used
- the setting around the historic building is respected and preserved wherever possible through the use of a carefully considered hard and soft landscape scheme
- proposals should have demonstrable regard to the guidance set out within the Conservation Area Character Assessment, Design Guidelines & Design Codes, and Policies DM46, DM47 and DM48 of the Local Plan Strategy Part 2.

Justification

It is important that historic buildings do not fall into disuse or disrepair and become neglected, as their value to the community can diminish. The Plan Area contains a number of historic buildings, and this policy seeks to encourage the re-use of these buildings for the benefit of both the plan area and the local community. The re-use of historic buildings can also act as a catalyst for the regeneration of an area, inspiring new development opportunities, improving the 'sense of place' and minimising waste through its re-use.

The National Planning Policy Framework sets out positive strategies to conserve the historic environment including those heritage assets considered to be most at risk due to neglect, decay, or other threats. This policy supports the re-use of those buildings most at risk.

The term 'historic buildings' is defined in this document to comprise:

- Listed buildings – listed by Historic England
- Significant non-listed heritage assets

The historic buildings in the plan area are above and the [NON LISTED HERITAGE ASSETS](#) are listed separately

Policy HE3 – The Battle of Rowton Heath

Any housing developments **must** not damage

-the setting of the Battle bounded by Hatton Heath to the south, the Shropshire Union Canal to the east and Manor Farm to the west

-the centre of the Battle bounded by the Shropshire Union Canal and Rowton Lane.

Justification

The Battle of Rowton Heath was the centre piece and determined the outcome of the English Civil War. The Battle of Naseby, 3 months earlier than Rowton Heath, is usually considered to have ended King Charles's hopes of winning the First Civil War, but Rowton Heath was one of the severest blows dealt to his prospects of continuing the struggle.

The battlefield area extends up to 400 yards on either side of Whitchurch Road with ground up to 500 yards deep at both ends of the lane (over which each side charged and counter charged the other as their opponents emerged from the lane's mouth) is considered to accommodate the battle. (English Heritage Battlefield Report: Rowton Heath 1645).

See the maps in [BATTLE OF ROWTON HEATH 1645](#)

Housing will seriously impact the historical context of the battlefield.

6.10 COMMUNITY FACILITIES

Provision and use of locally provided services by and for local people is of vital importance to the sustainability of the community. To be effective, local services and facilities are preferred to be centrally located for easy reach. The community is not able to pursue all the community activities that it would like either because the facilities are not available or are not appropriate.

Under the Localism Act 2011, existing community facilities, amenities and services can be protected by listing sites or buildings as assets of community value. It is important to identify and protect these especially when the loss of this asset would have a significant effect on the Community, in particular the youth and pre-school facilities it offers.

Policy CF1 - Existing community facilities

Planning applications for development which results in the loss or degradation of existing community facilities or services **must** demonstrate how they shall be replaced within the neighbourhood plan area by facilities or services of **equal or greater value** to the community to meet the evidenced need.

Policy CF2 – New or improved community facilities

Development of new or improved community facilities or services **must**:

- demonstrate they meet the expressed needs of the community
- provide improved and/or expanded access to local services and facilities for people living and working in the neighbourhood area
- prioritise the use of previously developed land

Policy CF3 - Community facilities and services required

- preschool outdoor space – the current use of the scout hut on the A41 is invaluable but does not provide an adequate storage for the activity and any space outdoors for the children.
- youth activities in in the Village, currently only the Scout Hut offers any facilities.
- more indoor spaces and activities would be useful to the local youth community such a Youth Club
- further 30 allotment plots attached to the existing site which is near fully occupied.
- 2-4 tennis courts, a crown bowling green and clubhouse to support the young and elderly

Policy CF4 - Community facilities not permitted

- mini supermarkets with noisy 24-hour deliveries
- fast food take -away outlets
- retail or service activities that bring significantly more cars into the village
- activities requiring new access points on the A41 or A51

List of existing community facilities and assets**Parish Hall**

Former site of Ring o’ Bells and then from 1884 the Boys School.

In 1974 leased to St James Parochial Church Council as a Parish Hall. Used by numerous local groups across several activities alongside St James’ events. Including Pilates, Art and Dance, History and RSPB Groups, the WI and Toddler Group.

St. James Church

Probable origins are Saxon with evidence a church existed in 1093. Perhaps, notoriously, it was used as a ‘Roundheads’ look-out post during the Civil War of 1644 to 1646. It exudes an air of peace and timeliness with its ancient tower, churchyard and lychgate. Supported by families in the Parish with a busy agenda of worship and community engagement.

The Methodist Chapel

Opened in 1888 and similarly the centre of worship and community activities, including Brownies, The Trefoil Guild, Parish Council meetings and especially *its “Sunday afternoon teas”*.

The Men’s Institute

The site was donated by Edward Porritt in memory of his son who was killed in the 1914-1918 world war. Healthy membership with licensed premises, snooker tables, meeting and social venue with amongst other things televised football matches!

The Women’s Institute

Frequented by’ the highly popular local Bridge Club on land donated by Edward Porritt’s wife.

The Scout Hut

Home for over 50 years to the vibrant Christleton companies of the Scouts, Guides, Christleton Pre-School, Irish dancing, Didi dance and Jujitsu. Currently the only venue offering facilities for young people.

The “Pit”

A major attraction both locally and for those from further afield. Volunteers known as the “Pit Group” regularly meet to keep the pond and surrounds at their best. A rich oasis for wildlife with ducks being the main attraction for children and the fish for those a little older. Sadly, the family of swans nesting there annually have in recent years passed away.

A Children’s Play area

Popular venue for families with wide-ranging equipment and a picnic location being a stone’s throw from the Pit. This has recently been upgraded to provide activities for a wide range of ages

King George V’s Playing Fields

Bequeathed to the parish for recreation purposes. Currently used for family picnics, all manner of ballgames, kite flying and an adjacent bike track.

Christleton Cricket Club

Adjacent to King George’s fields. The present pavilion was built in 1976 and the ground hosts both cricket and football matches across various age groups.

Christleton/Brio Sports Centre

Joint venture with Christleton High School. Offers both wet and dry facilities including a sports hall, 4G Astroturf football pitch, grass football pitches, swimming pool, fitness studio, gym, and with a selection of fitness classes. Restricted hours due to use by *the school*. *Available to book for children’s parties.*

Village store

An essential and very useful asset, unfortunately no longer a Post Office.

Ring of Bells Public house

Originally The Red Lion established in 1817, and sister Coffee Shop, Koow both well frequented and a great community asset.

Plough Inn Public House

Short walk outside the Village Centre, well frequented by residents offering the local Darts team, weekly quiz and regular meetings for various local groups. Meeting place for the local Cycle group.

Cheshire View

Venue available for weddings, corporate events and celebrations.

Costa Coffee drive through servicing walkers along the canal as well as passing traffic on the A41

Boughton Heath Allotments

Village Green, Memorial Shelter and Village Pump

Birch Heath Care Home

The Wyldewoods retirement village

As demonstrated, there are numerous well frequented Community Assets. The Ring o Bells, The Plough Inn, Christleton Sports Centre, St James Church, The Methodist Chapel, Children’s play area and Christleton Cricket Club all have small carparks to support them. However, the Village would benefit from Village car parks behind the Church and the High School at each end of the Village



6.11 LOCAL ECONOMY

Policy LE1 - Supporting homeworking

Proposals for the provision of accommodation for home working **must:**

- not compromise internal space standards, residential amenity, or highway safety
- provide suitable provision for new communication technologies including infrastructure and connections to high-speed broadband wherever possible
- not detract from the residential character of the area resulting from noise, increased vehicle movements and parking or deliveries to the business
- not detract from the residential character of the area or the individual dwelling concerned.

Justification

There is evidence that the proportion of homeworkers is growing both nationally and regionally and, following the Covid pandemic there has been a significant change in working patterns across the local area with more people choosing to work from home. Consideration needs to be given to the effects of such a major social shift on local amenity, neighbouring properties, and the highway network across the plan area. It is likely that this change in working patterns will continue in the future and that increased investment will be required in telecommunication infrastructure across the plan area to accommodate this change. Currently there are pockets within the plan area that have poor internet connection and mobile phone signal. The Parish Council encourages and supports improved provision across the whole of the plan area especially rural locations.

List of a some of the existing businesses

Rock House dental practice
 Drakes haircutters
 Christleton Sports Centre
 Matt Pate wedding photography
 Deva bridge club
 Urban reactive drainage service
 Carrs landscapes
 Marketing projects
 Barker building and architects
 Kieba Homes

AA decorative events
 Future online sales
 Christleton Orchard range
 Abbotts building
 The Village Store
 Ring O Bells, the Plough and Cheshire Cat pubs
 McCormick architects
 Chelma Graphics
 The Abbotts Well Hotel
 Barnhouse Veterinary
 Koow Coffee Shop
 Prodo Digital
 Oaklands Nursing Home
 Vicars Cross Golf Club
 Northern Healthcare (Beck House Tarvin Road)

Policy LE2 - Supporting small scale business development

The conversion of existing buildings for new small-scale employment and service-related business facilities **must**:

- provide safe access on and off the public highway for all vehicles including the provision of servicing and turning areas
- not lead to significant additional traffic having a significant adverse impact on the local highway network
- provide adequate vehicle and cycle parking for employees and visitors, appropriate provision of spaces for charging plug-in and other ultra-low emission vehicles
- ensure local residential amenity is protected, and suitable measures are put in place to mitigate and reduce to a minimum any noise or air pollution or other disturbance
- respect local character and distinctiveness

Justification

To ensure the continued success of the plan area it is important to provide new employment opportunities within it. New employment will attract additional people into the area and provide job opportunities for existing and new residents. Businesses will take an active role in improving and expanding local skill levels and reducing local unemployment. In the wider rural area, many businesses are agricultural or related to agriculture however there has also been a diversification into retail, leisure and lifestyle related activities with a number of local residents running businesses from their homes, especially where they are available to telework.

6.12 COMMUNICATIONS INFRASTRUCTURE

Policy C1– Broadband

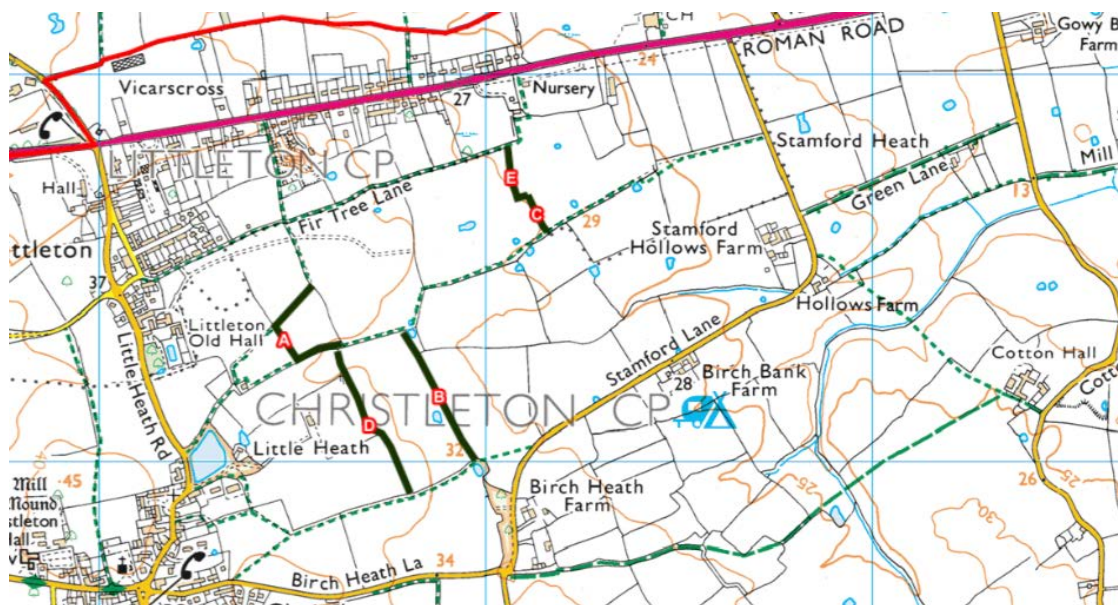
In areas where there is existing Fibre to the Cabinet (FTTC) provision, new developments **should** ensure that residential and business properties have Fibre to the Premises (FTTP) broadband. Major developments **should** provide FTTP broadband to all new properties. The development of an advanced high-quality communications infrastructure, including high speed, minimum 1Gbps capable, broadband and improved 4/5G mobile network coverage **must** be supported subject to

- visible street and aerial telecommunications development being kept to a minimum consistent with the efficient operation of the networks.
- all visible street cabinets or aerial development **must** be sympathetic to its surroundings and camouflaged where appropriate.
- urban and rural, residential and business, developments being equally supported.

6.13 PUBLIC RIGHTS OF WAY (PROW)

Introduction

The following Ordnance survey map illustrates the footpaths around the Village and includes the four new north-south footpaths (subject to enquiry) connecting Christleton with Littleton



Policy PROW1 – maintain PROW network around the village outlined in this plan

Policy PROW2 – convert the PROW from Huntington to Christleton into a shared user path and reinstate the footpath from the Canal to the A41

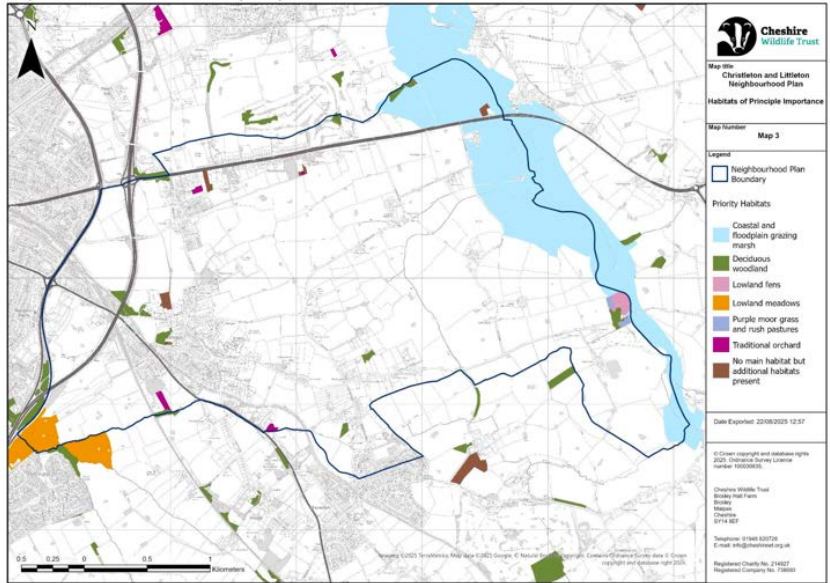
Policy PROW3 – any new developments must maintain and improve any paths within the development and introduce new paths to allow walkers and cyclists to access the village amenities without needing cars.

6.14 WILDLIFE AND BIODIVERSITY

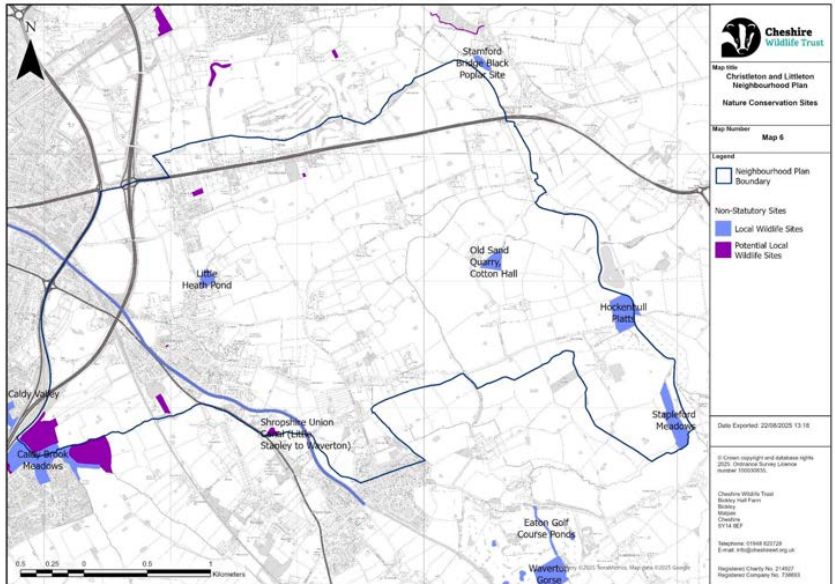
[CHESHIRE WILDLIFE TRUST REPORT](#) highlights that the important wildlife habitat in Christleton and Littleton is predominantly associated with the floodplain grazing marsh in the Gowy floodplain in the east of the Parish. The Caldý Brook Meadows area is also an important area of lowland meadows in the southwest, and the Shropshire Union Canal provides an important linear corridor.

The report recommends that any potential development proposals in the Neighbourhood Plan area **must avoid high distinctiveness habitats, core wildlife areas and the wildlife corridor network.**

1. Terrestrial Habitats of Principal Importance



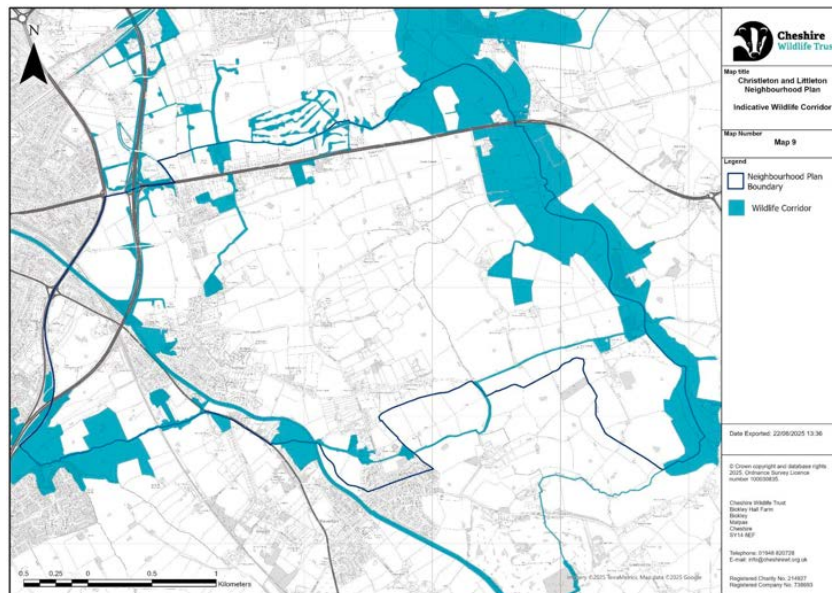
2. Designated Sites of Nature Conservation



3.Habitat Distinctiveness



4.Indicative Wildlife Corridors



Policy CWT 1 - Wildlife and Biodiversity

Development proposals on land that lies adjacent to a designated High Distinctiveness Habitat (as shown on map3) or on land that adjoins the 15m buffer of a designated Wildlife Corridor **must** avoid or minimize potential harmful impacts to wildlife, and where avoidance is not feasible, proposals must demonstrate satisfactory mitigation measures. Proposals on land that lies in both types of location **must** be supported by a proportionate drainage strategy demonstrating the protection of sensitive environmental assets.

Development proposals that may affect habitats that are dependent on hydrology (e.g. running or standing water or floodplains) that are not supported by such a drainage scheme **will be resisted**.

Any development adjacent and **likely to have an impact** on those areas highlighted in the plans set out above **must** incorporate substantial mitigation to minimise the residual effects on wildlife while also seeking to enhance the overall condition of habitats in order to achieve a measurable net-gain for biodiversity.

This can be achieved by:

- Prioritising a scheme design that retains and enhances important semi-natural habitats and key features for biodiversity, while also improving the permeability and function of the site for wildlife by creating new resources within and new connections to the wider landscape.
- Embedding out of bounds areas and dark corridors along watercourses, woodland edges and hedgerows into the environmental design of the scheme.
- Ensuring all external lighting is directional, low spillage and wildlife friendly.
- Ensuring the scheme drainage strategy directs run-off away from sensitive environmental assets and does not promote pollution propagation pathways. This is particularly important for habitats that are dependent on hydrology such as running or standing water, peatlands, and floodplain grazing marshes.
- Incorporating Sustainable Drainage Schemes (SuDS) which can provide additional wildlife habitat, provide measurable net-gains for biodiversity and prevent flooding. However, SuDs may hold polluted water so should not drain directly into running or standing water unless an extensive filtration or settlement system is in place.
- Ensuring only UK and Northern Ireland sourced and grown nursery stock of native plant and tree species be used in the landscaping of developments.
- Incorporating species specific mitigation measures where appropriate such as: Hedgehog-friendly fencing, purposely designed to allow the passage of hedgehogs from one area to another;
- South facing banks or bunds for reptiles, butterflies and other invertebrates, and;
- Bee bricks and bat or bird boxes, ideally made of durable material such as woodcrete.

Justification

Biodiversity net gain will become increasingly important during the lifetime of this plan and where Biodiversity Net gain is levied within the plan area consideration should be given first to investing in local projects that benefit the local area.

Policy CWT 2 - Wildlife Corridors

Development proposals on land which lies within 15 meters of a Wildlife Corridor or Site of Designated Nature Conservation (see plans above) will be permitted provided they safeguard and enhance the elements for which the Corridor is identified and defined.

Enhancement of wildlife corridors may be facilitated by opportunities arising through the planning process (e.g. BNG or other ecological compensation via Section 106 Agreements or Planning Conditions), through government incentives (such as agri-environment schemes) or through the aspirations of the local community working with local businesses and landowners.

Justification

The natural environment is fundamental to our wellbeing, health and economy providing a range of ecosystems such as water, food, flood defences and carbon sequestration. Biodiversity underpins most, if not all, of these systems. The pressure on our natural resources is likely to continue and increase and we need to manage these resources in ways that deliver multiple benefits across the plan area.

There are other opportunities to enhance the wildlife corridors across the plan area such as those set out in the UK Government England Trees Action Plan.

(However, it is vitally important that tree planting should only occur on species-poor habitats away from existing (non-woodland) priority or semi-natural habitats, watercourses or aquatic habitats such as ditches and ponds and any other habitats of value to specific wildlife. Specialist ecological advice should always be sought before any tree planting is undertaken to ensure no unintended negative effects to biodiversity arise as a result.)

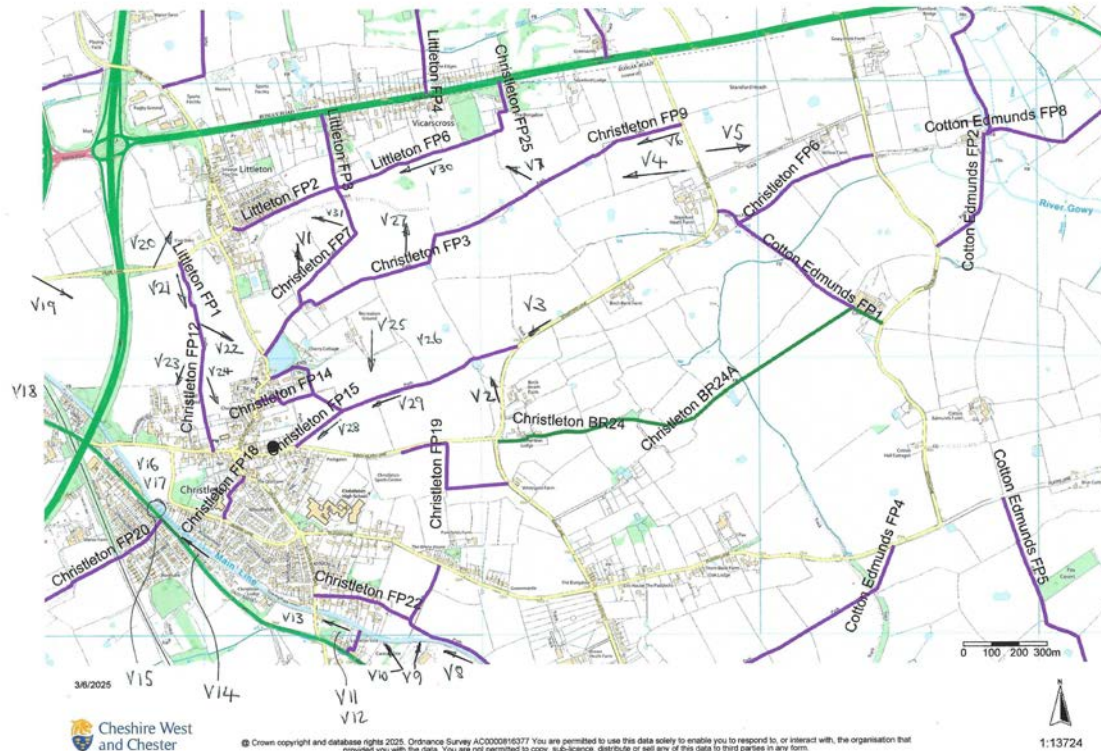
Further detail is available in the following [WILDLIFE BACKGROUND DOCUMENT](#)

A [WILDLIFE SURVEY](#) provides a 2025 snapshot of wildlife sightings in the Villages of Christleton and Littleton

6.15 LOCAL VIEWS

Policy LV1 - Landscape quality, countryside and open views

All new development **must** respect and enhance significant local views and vistas and ensure that significant local skylines are maintained and where possible, enhanced and protected from development. Development proposals **must** not, individually or cumulatively, significantly harm locally important characteristic features.



Here are photographs of the [CHRISTLETON AND LITTLETON VIEWS](#)

Justification

The parishes of Christleton and Littleton are in a rural area comprising two settlements together with a number of scattered houses and farmsteads. The plan area is primarily rural and characterised by agricultural land, mature trees and hedgerows. The two settlements contain a number of important open views and local vistas that are integral to the structure and fabric of the area and the settlements within it. The local communities consider these open views and vistas very important and have indicated that wherever possible they should be maintained and enhanced.

6.16 INFRASTRUCTURE

Policy INF1 - Sewerage and Drainage

Any net increase in wastewater generation and the impact on the local sewerage and drainage works **must** be carefully considered in any new development proposal. New development will only be supported where

-there is insufficient capacity in the local sewerage system – therefore any planning application, for a substantial number of houses in Christleton or Littleton requiring waste water treatment relying on the Chester WwTW, **must** be refused and **must not be occupied** until Welsh Water have provided additional waste water treatment capacity

-it can be demonstrated that surface water drainage **must** not add to existing site run-off, enter the foul-combined sewer network, or cause any adverse impact to neighbouring properties or the wider environment

-any proposed connection **must** not increase the risk of system back up/flooding or cause adverse impact to the neighbourhood plan area.

-all development proposals **should** incorporate Sustainable Drainage Systems (SuDS) with run off rates no greater than greenfield sites. The design of any SuDS should be tailored to the landscape character of the area and, where possible, contribute towards the landscaping and biodiversity of the development together with provision for future maintenance.

Justification.

Justification

Paragraph 182 of the NPPF 2024 states that applications which could affect drainage on or around a site should incorporate sustainable drainage systems to control flow rates and reduce volumes of runoff proportionate to the scale of the proposals. These should provide multifunctional benefits wherever possible, through facilitating improvements in water quality and biodiversity as well as benefits for amenity. Sustainable drainage systems provided as part of proposals for any major development should;

- take advice from the Lead Local Flood Authority
- have appropriate minimum operational standards, and
- have maintenance arrangements in place to ensure an acceptable standard of operation for the lifetime of the development.

In respect of a 200 house development on the A41 Whitchurch Road, Welsh Water stated on Sept 22 2025

'The proposed development site is located in the catchment of a public sewerage system which drains to Chester Wastewater Treatment Works (WwTW). We have considered the impact of foul flows generated by the proposed development and concluded it is unlikely that sufficient capacity exists to accommodate the development within the immediate public sewerage system without causing detriment to the existing services we provide to our customers, or in regard to the protection of the environment'

7. PARISH COUNCIL ACTIONS

In addition to the planning policies set out in this Neighbourhood Plan the following air quality, flooding, highways/traffic and cycling initiatives are required to support sustainable development in Christleton and Littleton.

Any lack of adequate highways infrastructure, air quality mitigation, waste water treatment, drinking water, flood mitigation and other critical infrastructure must prevent housing development in Christleton and Littleton.

In theory a Grampian condition can be used to prevent a development from being occupied until the infrastructure is in place. A Grampian condition is a negatively worded planning conditions that prevent the commencement of a development until certain actions, often off-site works, are completed. These actions typically involve land not controlled by the applicant and may require the consent or authorization of another party. In practice a Grampian condition is difficult to enforce as once the development is built the developer will try to get around the condition.

If obvious infrastructure limitations around Christleton and Littleton are not resolved and taken into account in planning application decisions, to the satisfaction of the Parish Councils, then it their intention to enforce appropriate action through legal routes.

7.1 AIR QUALITY

About 4 million people die globally each year as result of outdoor exposure to particulate air pollution. In Europe, 520 000 excess deaths a year were attributed to air pollution in 2017. In the UK 40 000 deaths a year are attributed to air pollution with London alone contributing 10 000 deaths. These deaths are mainly due to exposure to nitrogen dioxide (NO₂) and fine particles (PM 2.5)

The levels of pollution identified in the attached Air Quality Working Group (AQWG) report on the A41 and through Christleton Village centre to the A51 is comparable to larger urban locations like Bolton, Bournemouth, Leicester, Edinburgh, Manchester, Newcastle, Liverpool, Bristol to name just a few of over 70 locations throughout the UK [AIR QUALITY REPORT](#)

The fine particles (PM2.5) are generated from the tyres and disc brakes on vehicles. The conversion to electric drive, and the batteries required, results in a substantial increase in the weight, wider tyres and larger disc brakes. Therefore, electric cars may generate more fine particles than their predecessors.

The Parish Council are therefore concerned that pollution of the A41/51 and the centre of Christleton will continue for many years

Actions

The actions that must flow from the AQWG report [AIR QUALITY REPORT](#)

AQ1-minimise the number of children and elderly people walking along or crossing the A41/51 by providing non polluted alternative routes where possible

AQ2-Request S106 funding to ensure continuous monitoring and reporting of PM 2.5 and NO₂ on the A41/51 and in the centre of Christleton

AQ3-extend the Chester Air Quality Management Area (AQMA) to include the A41/51 as PM2.5 and NO₂ levels are higher than 77/91% of measurements inside the existing AQMA

AQ4 – Ensure that all new highway surfaces are sealed surfaces that do not abrade and hold particles on the A41/51, adjacent to housing, and in the centre of the Village.

Failure by the Highway Authority to implement these actions will result in fine particles penetrating deep into the lungs and cardiovascular system of Christleton and Littleton young children and elderly residents with the long-term consequences of strokes, heart disease, lung cancer, asthma and respiratory infections to name just a few

7.2 FLOODING

Introduction

Current rainfall levels are three times what they were 30 years ago. As a result, flooding is becoming a regular occurrence during high rainfall. In most circumstances given some time the water drains away however there are four locations(F2-6) listed below that seriously impact the village. Any new developments must contribute to resolving these three problems as well as ensuring that the developments do not introduce further flooding in new locations.

Actions

F1-Any developments in Christleton and Littleton must make an S106 contribution to the elimination of the anticipated flooding adjacent to the development and at the following locations

F2-Flooding along Plough Lane that prevents Waverton schoolchildren from accessing the High School

F3-Flooding at the junction of Village Road, Plough Lane and Quarry Lane, in front of the High School, that regularly becomes polluted with back flow of waste water and prevents access to the High School

F5-Flooding of the Primary school car park and footpath that prevents all schoolchildren from accessing the Primary School

F6-Flooding of Pepper St outside the entrance of the Law College that prevents Great Boughton schoolchildren from accessing the High School

F7-Porous surfaces must be provided for all developments that naturally drain rainfall away and not on to adjacent properties

Justification

Failure to take action will result in children being unable to access to the Primary and High schools as well as a dislocation of transport in and out of Christleton

7.3 HIGHWAYS AND TRAFFIC

Introduction

Christleton and Littleton are bounded to the west and the north by the A 41 and A51. The hamburger junction at the end of the A41 and the traffic lit roundabout at the end of the A51 is overloaded at peak times in the morning and in the evening rush hours. As a result, there are queues which can be anything up to half a mile long. The consequence is that vehicles try to avoid these queues by cutting through Christleton from the A41 to the A51 and the A51 to the A41. This cutting through traffic represents half of all the overall traffic going through the centre of Christleton.

In addition, the High School (1400 pupils) and Primary (200 pupils) and all the staff arrive and leave the school over a period of 25 minutes. These intense waves of pedestrians, cyclists and cars, if continued throughout a 24hour period, would be equivalent to movement of more than the population of Chester!

Actions

The Parish Council will work with the Local Highway Authority on the following matters;

A41/51 improvements

HT1-Signage to direct traffic via M56/M6 rather than down the A41 and A51

HT2-Implement throughput improvements to the hamburger junction on the A41 and the traffic lit roundabout at the end of A51(consider switching lights off all or some of time) so as to reduce congestion and queueing on the A41 and A51 which contributes to high PM2.5, NO2 and injury risks along the A41/51 as well traffic cutting through Christleton at peak times to avoid the queues on the A41 and A51

HT3-Prevent any new accesses and high traffic generative businesses on or off the entire length of the A41/A51 and through the centre of Christleton

A41 improvements

HT4-Increase the width and replace the surface of the canal footpath so as to move as many Huntington and Great Boughton schoolchildren and elderly pedestrians and cyclists as possible off the polluted and dangerous A41 shared user path and on to the canal footpath

HT5-Widen the shared user path over the railway with an independent truss bridge or as part of a project to replace the railway bridge

A51 and Littleton improvements

HT6-Reduce speed limit from Littleton to Christleton to 30mph, on the A51 to 20mph and carry out an assessment of the benefits of an average speed camera system on the A51

Centre of Christleton

HT7-Restrict any new vehicular accesses and high traffic generative businesses on or off Little Heath Road, Village Road and Rowton Bridge Road

HT8-Reduce vehicular speeds, congestion and improve active travel provision through incremental implementation of the [CHRISTLETON VILLAGE CYCLING WALKING AND TRAFFIC PLAN](#) designed by a Village Working Group, CWAC Highways and Planit

HT9-incremental implementation of the [SOUTH EAST CHESTER CYCLING PLANS](#) using the proposed prioritisation

HT10- alleviate congestion at school arrival and leaving times by removing pinch points by continuing extension of H bars to provide passing places

HT11-continue to promote 'Park and Stride' locations at Costa Coffee, the Plough Inn, Pearl Lane and the Cheshire Cat for children to be dropped off on the periphery of the Village

HT12-continue a policy of no or minimum road markings to enhance the appearance of Village centre

HT13-if and when the High School is replaced by a new building at the other end of the site, consider providing a main access point to the rear of the school from Birch Heath Lane or Rake Lane so as to eliminate gridlocks and congestion in the Village centre

Parking

HT14-provide additional car parking capacity behind St James's Church and behind the High School as and when the opportunity arises

HT15-provide badge access to these new car parking spaces for members of the bridge club so as to reduce congestion on Village Road at school arrival and leaving times

HT16 -comply with up-to-date parking standards for width of new spaces Cheshire West and Chester Parking [PARKING SPACE STANDARDS 2022](#)

Justification

Continued failure to divert HGV traffic down the M56 and M6, make throughput improvements to the A41/51, divert school traffic to the rear of a new High School and provide additional parking in the centre of Christleton will continue to make a major contribution to the adverse PM 2.5 health implications and accident injury rates on the A41, A51 and in the centre of Christleton

7.4 CYCLING PLANS

The Parish Council carried out a [TRAVEL SURVEY](#) of all 1400 children in Christleton High School 47% of the children travel from Boughton, great Boughton and Saughton, 21% of the children travel from Christleton, Littleton and Waverton and the remainder travel from all over Chester.

The children use the following travel modes

Car	33.5%
Park and Stride	4.9%
Bus	7.1%
Cycle	17.2%
Walk	37.4%

The long-term objective is to reduce car use from 33.5% to 10% as the peak use of cars use to/from the schools cause congestion, gridlocks, pavement mounting and high PM2.5 levels in the centre of Christleton and to and from Littleton and the adjacent villages. A third of the children, particularly Great Boughton and Huntington children, feel that their route to school is 'unsafe or dangerous' which increases car use and congestion on the A41 and in the centre of Christleton village.

The [CHRISTLETON CYCLING WALKING AND TRAFFIC PLAN](#) addresses these issues within Christleton Village so as to ensure the Village is cycling and walking friendly. The High School [TRAVEL SURVEY](#) was then used to identify key improvements in cycling access to and from Christleton.

A team of volunteers representing all the surrounding parishes was assembled and they produced the [SE CHESTER CYCLING PLANS](#) for each parish and for connections between each of the

Parishes. This was then professionally prioritised [SE CHESTER CYCLING PLAN PRIORITIES](#) and given to CWAC. These priorities require funding with help from S106 contributions from all future housing developments

CP1-Painted footpath and coloured tarmac on Rowton Bridge Road – Dutch style pedestrian and cycling priority to discourage car access

CP2-Provide truss bridge to widen SUP across the A41 railway bridge

CP3-Resurface Canal footpath from Rowton Bridge Road to Great Boughton

CP4-Improve safety at the high school entrance by separating vehicles, pedestrians and cyclists

CP5-Incrementally fund and complete Planit cycling, walking and traffic plan for the centre of Christleton

8 GLOSSARY

Amenity– A positive element or elements that contribute to the overall character or enjoyment of an area.

Ancient woodland – An area that has been wooded continuously since at least 1600AD. It includes ancient semi-natural woodland and plantations on ancient woodland sites (PAWS).

Archaeological interest – There will be archaeological interest in a heritage asset if it holds, or potentially holds, evidence of some past human activity worthy of expert investigation at some point.

Biodiversity – A measure of the number and range of species and their relevant abundance in a community.

Biodiversity Net Gain - is the name given to a process of biodiversity offsetting whereby any habitat loss caused by a new development is mitigated to ensure an overall increase in biodiversity post-development. As part of initial development plans, developers are increasingly required to demonstrate that biodiversity loss is mitigated. To establish the best way to protect the habitats on-site, a metric is used. To achieve biodiversity net gain, a development must show evidence that a development will increase the biodiversity value of a site by at least 10%. This statistic is presented in the value of a biodiversity net gain metric called ‘biodiversity units.

If habitats and ecological value cannot be added to the site itself, then mitigation plans must be put into place to ensure habitats. This can be achieved either by enhancing the on-site biodiversity or creating new habitats elsewhere.

BREEAM - BREEAM sets the standard for best practice in sustainable building design, construction and operation. It is a globally recognised standard and allows the assessment and benchmarking of new and existing property assets across a multitude of building types. A BREEAM assessment uses recognised measures of performance to evaluate a buildings specification, design construction and use across:

- Energy and water use
- Internal environment (health & wellbeing)
- Management processes
- Pollution
- Transport
- Materials
- Waste
- Ecology

Cheshire West and Chester Council– The local authority for the plan area.

Community– At its largest it means all of the communities in the Plan area.

Community Facilities– Facilities providing for the health, welfare, social, educational, spiritual, leisure and cultural needs of the community.

Community Infrastructure– The basic facilities, services and installations needed for the functioning of a community or society. It includes community buildings and halls, leisure facilities, cultural facilities, education services, healthcare facilities and renewable energy installations.

Community Infrastructure Levy (CIL) - Allowing local authorities to raise funds from owners and developers of land undertaking new building projects in their area.

Consultation Statement (Statement of Consultation) – A statement accompanying the Neighbourhood Plan. The statement must set out what consultation was undertaken and how this provided the basis of the NP.

Core Strategy – A Development Plan Document prepared by Cheshire West and Chester Council setting out the long-term spatial vision for the wider area and containing both strategic and generic policies which will apply to all development proposals in the local authority area.

Countryside – The area outside the settlement boundaries.

Development – Defined under the 1990 Town and Country Planning Act as “the carrying out of building, engineering, mining or other operations in, on, over or under land, or the making of any material change in the use of any building or other land”. Most forms of development require planning permission.

Development Plan – A plan comprising the Development Plan Documents contained within the Local Development Framework. This includes adopted local plans and neighbourhood plans and is defined in Section 38 of the Planning and Compulsory Purchase Act 2004.

Enabling development - is development that would be unacceptable in planning terms but for the fact that it would bring public benefits sufficient to justify it being carried out, and which could not otherwise be achieved. The key public benefit to significant places is usually the securing of their long-term future.

Evidence Base – A researched, documented, analysed and verified basis for preparing the Neighbourhood Plan. It consists of many documents produced over a period of years by the local authority as part of the process of developing its Core Strategy.

Evidence Base Summary – A document produced as part of the process of developing the Neighbourhood Plan. It supports the plan by setting out a summary of the relevant Evidence Base and explaining how decisions were made as to where new development should be located in the NPA.

Examination – An independent review of the NP carried out in public by an Independent Examiner.

Green Corridors – Green spaces that provide avenues for wildlife movement, often along streams, rivers, hedgerows, or other natural features, which connect green spaces together.

Greenfield – Land on which no development has previously taken place.

Heritage asset – A building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions because of its heritage interest. It includes designated heritage assets and assets identified by the local planning authority. (local listing)

Independent Examiner – Anyone with appropriate qualifications and skills and who meets certain requirements set out in the Localism Act. This could be a planning consultant or other planning professional, an employee of another local authority or a planning inspector.

- is in keeping with the scale, character and appearance of its surroundings and the local area;
- does not give rise to unacceptable impacts; and
- does not involve the loss of undeveloped land that makes a positive contribution to the character of the area.

Infrastructure – All the ancillary works and services which are necessary to support human activities, including roads, sewers, schools, hospitals, etc.

Local – Within our neighbourhood plan area.

Local Green Gaps – Areas that support the distinctiveness of settlements and that are identified in neighbourhood plans. LPS policy PG6 ‘Open Countryside’ will apply and development will not be permitted where erosion of the physical gap between settlements is identified, where the visual character of the landscape is adversely affected and/or where development may lead to the coalescence between or within existing settlements.

Local Green Space – areas of local green space within the settlements in the neighbourhood plan area that are valued and used by the local community for a variety of purposes.

Local Housing Needs – Housing that generally meets the needs of existing residents of the NPA or with a local connection or in the case of essential agricultural dwellings, employed or last employed in agriculture. Evidence of need should be based on the latest Parish Housing Needs survey.

Local Wildlife Sites – Sites with ‘substantive nature conservation value’, they are defined areas identified and selected locally for their nature conservation value based on important, distinctive, and threatened habitats and species with a national, regional and local context.

Mitigation Statement – The best effort to minimise significant harm that it is deemed cannot be wholly or partially avoided.

National Planning Policy Framework (NPPF) – Published by the Government in July 2018, and subsequently amended a number of times, the latest version published in December 2024, sets out the Government’s planning policies for England and how these are expected to be applied.

Nature Improvement Area - Nature Improvement Areas (NIA) were established to create joined up and resilient ecological networks at a landscape scale. They are run by partnerships of local authorities, local communities and landowners, the private sector and conservation organisations with funding provided by the Department for the Environment, Food and Rural Affairs (Defra) and Natural England

Neighbourhood Plan – The full title in the Localism Act is ‘Neighbourhood Development Plan’. It is a document for a defined area, subject to examination in public and approval by local referendum. It will be used in the determination of planning applications.

Open Space – All spaces of public value, including public landscaped areas, playing fields, parks and play areas, and areas of water such as rivers, canals, lakes and reservoirs, which may offer opportunities for sport and recreation or act as a visual amenity and a haven for wildlife.

Passive house - Voluntary standard for energy efficiency in a building that reduces the building's carbon footprint. Conforming to these standards results in ultra-low energy buildings that require less energy for space heating or cooling.

Plan Period – The period for which the Neighbourhood Plan will set policy for the NPA. This will be from the adoption of the plan from 2026 until 2030 by agreement between Combined Parishes and Cheshire West and Chester Council

Referendum – A general vote by the electorate on a single policy question that has been referred to them for a direct decision. In the case of the NP, the referendum will decide whether or not to adopt the plan.

Residential Amenity – The quality of the living environment for occupants of a dwelling house including its associated external spaces.

Rural Area - The part of the NPA area outside identified settlements.

Rural Exception Sites - Small sites used for affordable housing in perpetuity where sites would not normally be used for housing. Rural exception sites seek to address the needs of the local community by accommodating households who are either current residents or have an existing family or employment connection.

Sensitive Development – Blends with and is complimentary to existing properties in all aspects of footprint, shape and height, incorporating some of the local features with regards to materials, window design, roof pitch etc.

Settlement Boundary - A settlement boundary defines the limits of development and makes clear where development will and will not be allowed, regardless of other constraints. The purpose of settlement boundaries are: -

- To direct future housing, economic and community related development in the Neighbourhood Plan Area to the existing settlement of Weston and to enhance its role as a resilient and sustainable community and to protect the surrounding open spaces and countryside.
- To contain the spread of the settlement, by reinforcing the core area and maintaining an effective and coherent built-up rural edge.
- To ensure that proposals for new housing development outside the settlement boundaries will only be granted in accordance with Policy H1 or in exceptional circumstances, such as any new dwelling required for the essential need of an agricultural worker to live permanently on or near their place of work in the countryside; and
- To preserve the existing and proposed green gaps which provide separation from other adjoining settlements and strategic allocations.

Settlement characteristics – features that define a settlement, including its site, situation, function, and pattern. The site is the actual location, while the situation describes the settlement's location relative to its surroundings. Settlements can have various functions, such as residential, industrial, or commercial. They can also be dispersed, linear, or nucleated in pattern.

Setting of a heritage asset – The surroundings in which a heritage asset is experienced, its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral.

Spatial integrity of settlements refers to the organized human habitation and the spatial patterns of their structures. Settlements can be urban, rural, compact, or dispersed, each with unique characteristics.

The spatial arrangement of settlements can be analysed to identify patterns and understand their relationship to infrastructure, economic, or political conditions.

Sustainability Appraisal – A process of appraising policies for their social, economic and environmental effects, which must be applied to all Development Plan Documents.

Steering Group – A group of local people representing the Parish Council, community groups and businesses that collated and guided the work on the NP.

Strategic Environmental Assessment – Assessments made compulsory by the European Directive (the SEA Directive). To be implemented in planning through Sustainability Appraisals of Development Plan Documents and NPs where required.

Sustainable transport modes – Any efficient, safe and accessible means of transport with overall low impact on the environment, including walking and cycling, low and ultra-low emission vehicles, car sharing and public transport.

Sustainable Urban Drainage Systems (SUDS) - A drainage system that controls the rate and quantity of run-off of surface water from developments.

Wildlife Corridor – Areas of habitat connecting wildlife populations

9 NEIGHBOURHOOD PLAN STEERING GROUP(NPSG)

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Lauren Buckley	Eirlys Smalldon
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Andy Foxall	
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On behalf of Christleton and Littleton Parish Councils